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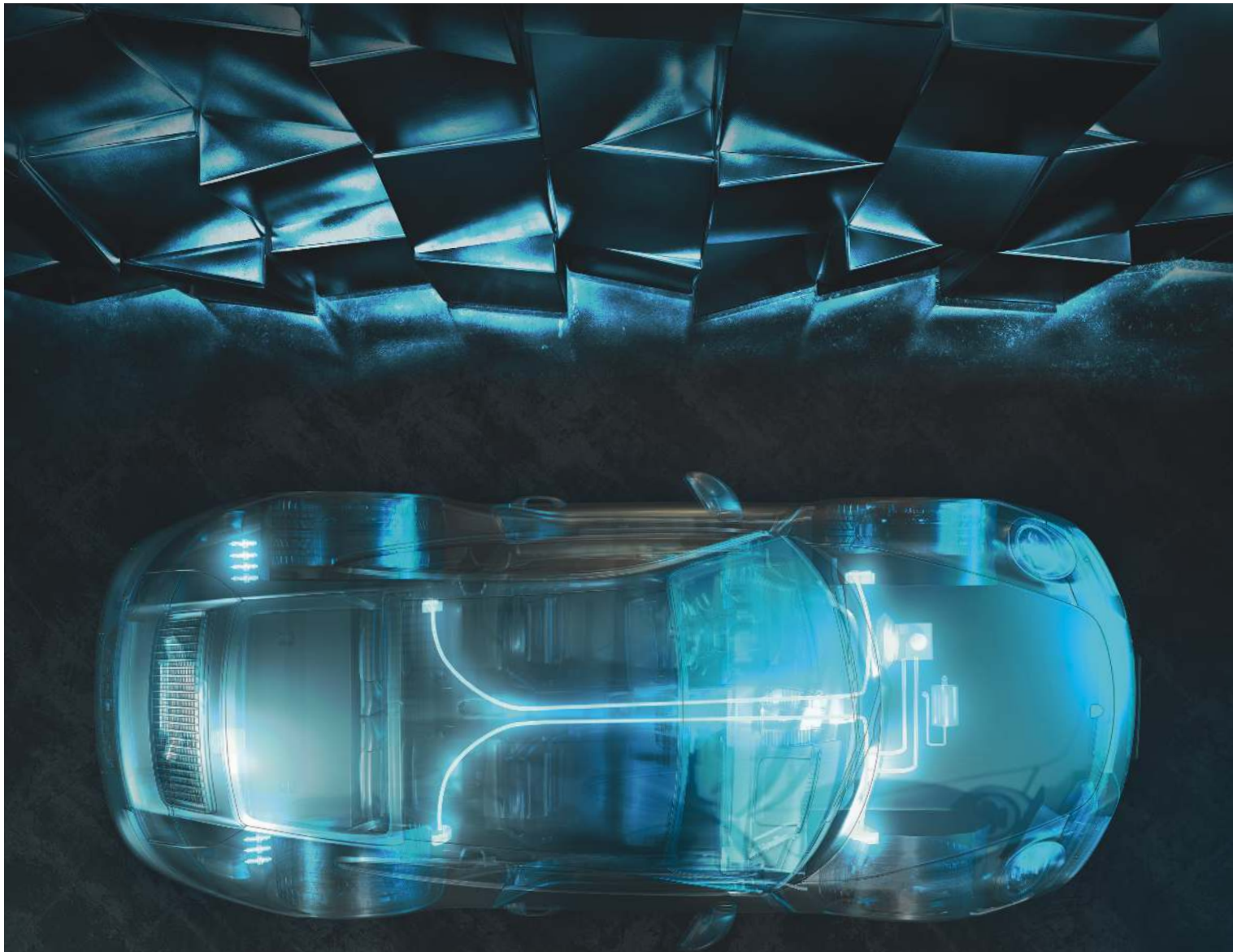
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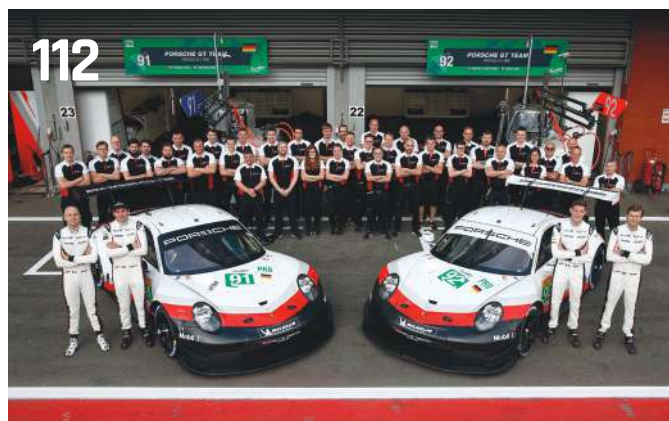
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MOVING FORWARD



Change. It's true to
say not everyone is
a fan. We only have
to look back to the
introduction of the
996 to see how a
high number of
those in possession
of an air-cooled 911

were up in arms about the introduction of
a water-cooled incarnation of our favourite
manufacturer's flagship offering. "It's not a
proper Porsche," they cried. The same words
were fired in the direction of the Stuttgart
brand's immensely popular transaxle range
of cars. The Boxster, Cayenne, Panamera and
even the 991 have also been on the receiving
end of criticism from many who would
prefer time to stand still.

Like it or not, as the years roll by,
Porsche's product lines have to evolve. New
technologies, increasingly strict emissions
regulations, customer expectation. These
are factors all car makers have to take into
consideration if they wish to maintain or
increase market share. Add to that the fact
a radical rethinking of Porsche's output has
been an essential exercise in ensuring the
survival of the company on more than one
occasion, and it soon becomes clear a change
of approach to the way an organisation

designs, delivers and promotes its products
is often necessary to broaden appeal with a
view to promoting continued success.

The same is true of *GT Porsche*, which
I'm thrilled to have been asked to take on
as editor. Some of you will recognise me
from the time I've spent managing other
premium marque titles, but don't be fooled
into thinking a new look and a fresh editorial
tone means "the world's premier Porsche
magazine" is about to forget its illustrious
past. As the very first issue of *GT Porsche*
proclaimed back in December 2001, revisiting
classic models and showcasing the best of
what's new is, and will always be, at the heart
of what this popular Porsche publication
is about. Even so, time has moved on, and
with it, the print industry has experienced
unprecedented change. On an increasingly
competitive newsstand, magazines can ill
afford to rely on past success as a guarantee
of continued good fortune. At various times in
its colourful history, Porsche recognised the
same with its product range. The resulting
change of tack wasn't always met with
universal approval, but the health of the
brand today is all the better for it.

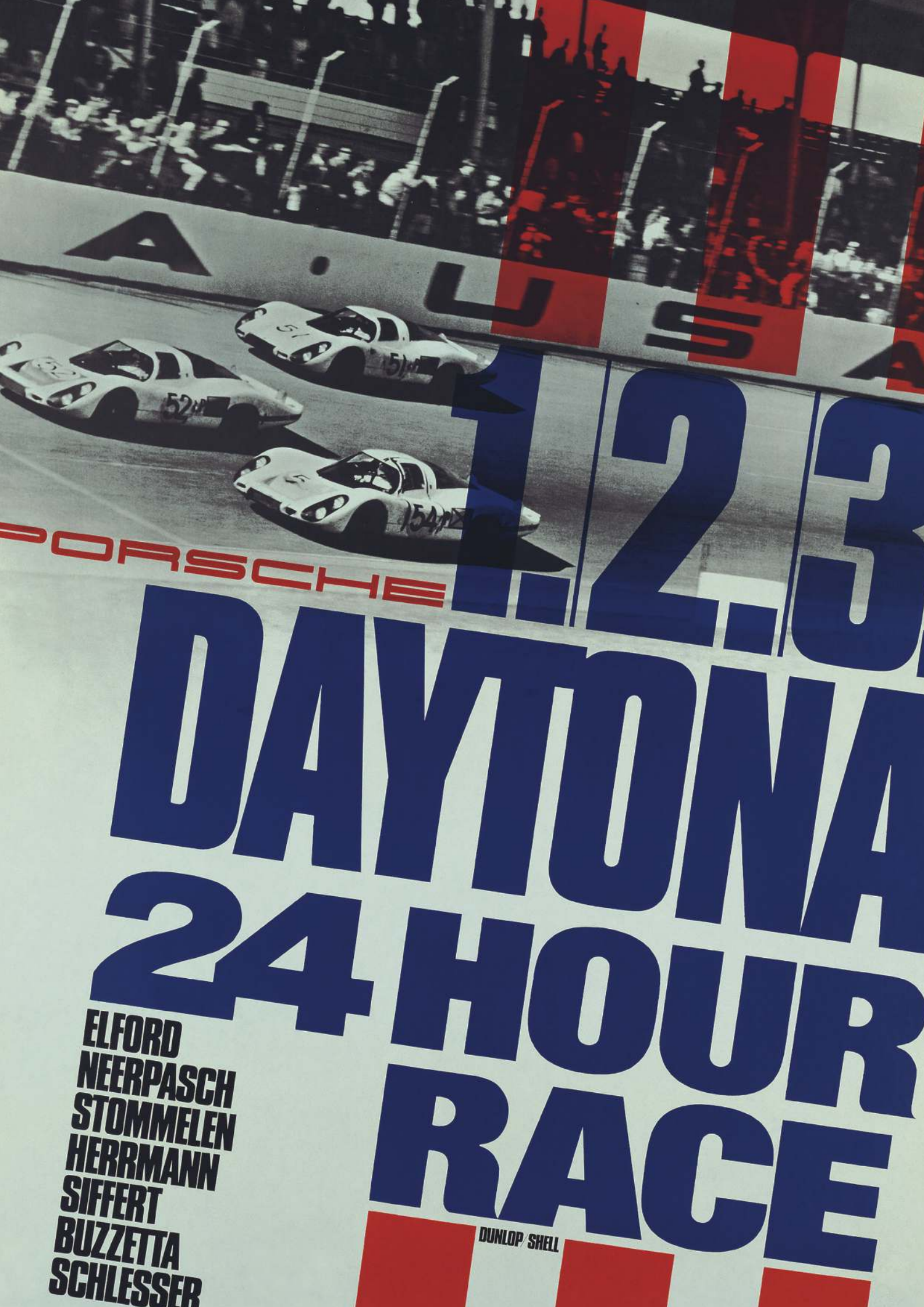


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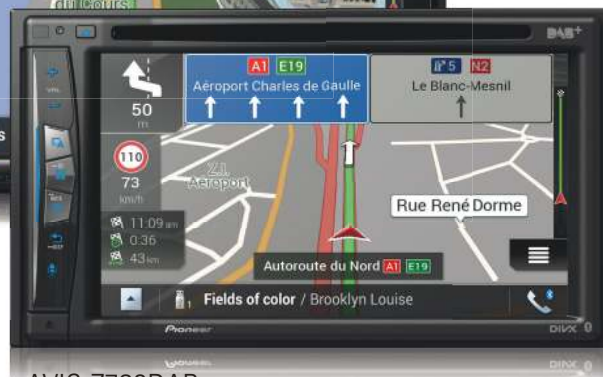
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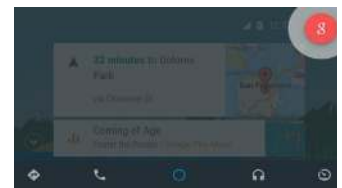
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997.2 TURBO



CLOUD NINE

With advanced chassis dynamics and 570bhp on tap, this modified 997 Gen II Turbo ably demonstrates why the 996's successor continues to enjoy heightened popularity

Words **Dan Furr** Photography **Dan Sherwood**

When Porsche commenced manufacturing of the 997 in 2004, 911 enthusiasts everywhere exhaled a sigh of relief. The 997's predecessor, the 996, was arguably the most controversial production model in the company's history, signalling the 911's switch from air-cooled to water-cooled engines, plus a radical rethinking of the way the Stuttgart brand's flagship offering should look. A revised profile, 'fried eggs' in place of 'bug eyes', generously proportioned bumpers and an updated interior suggested revolution more than evolution, yet if complaints Porsche purists levelled at the new 911 were loud when the first 996s rolled off the production line in 1997, they were positively drowned out by the later chorus of screaming from unlucky owners finding the mechanical componentry propelling their Porsche was susceptible to catastrophic failure. To add insult to injury, the quality of materials used for the new upholstery and exterior decoration was likely to wear prematurely. Even the 996's in-car entertainment system earned a reputation for playing up.

Of course, not all 996s are basket cases, and in an age of online forums and social media posts spreading bad

news like wildfire, it's safe to assume the number of owners experiencing the difficulties we've just outlined is exaggerated as a result of horror stories published online gaining far more traction than tales of hassle-free motoring. Add the preventative measures owners can take to ensure their prized Porsche continues to deliver plenty of smiles to the mile, and it quickly becomes apparent the much maligned model's less than enviable reputation has had the positive effect of ensuring there are bargains to be had on the used car market, making 911 ownership affordable to those working with a modest budget. Even so, high profile tales of engine failure are enough for many prospective purchasers to dismiss the idea of investing in a 996, and, if we're being honest, most buyers want their 911 to exhibit classic 911 DNA, hence the immense popularity of the 997 following Porsche's decision to return to familiar design cues.

GENERATION GAME

"The 997 is a great car, but it's not a 911 without its problems," says Peter Robinson, General Manager at independent Porsche specialist, Ninemeister. He's hinting at the 997's main developments being »

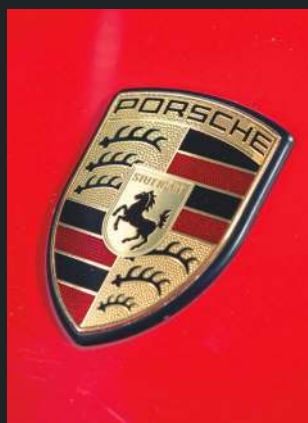
Facing page
For many 911 enthusiasts, Guards Red paintwork accompanied by black five-spokes and polished rims is an unbeatable look

IT'S THE 997'S WILLINGNESS TO BE TWEAKED AND TUNED THAT LED US TO NINEMEISTER'S WARRINGTON WORKSHOP

CHANGE GEAR

The 997 was given a facelift in 2008. Styling updates were accompanied by revised suspension. PDK dual-clutch transmission replaced the Tiptronic system.





interior and exterior styling, with the model's early underpinnings remaining largely the same as those of the late 996. "Second-generation 997s are widely regarded as being more desirable than earlier examples thanks to a revised drivetrain and uprated chassis equipment," he says, "but whatever 997 you find yourself driving, rest assured you're in control of a 911 that's receptive to modifying."

MIX AND MATCH

It's the 997's willingness to be tweaked and tuned that's led us to Ninemeister's Warrington workshop, where the company's talented technicians have just finished working their magic on a 997 Gen II Turbo. It's a gorgeous car, blending retro styling with present-day levels of performance and efficiency. "The wheels are genuine Sport Classic staggered nineteens," confirms Peter. Coupled with the deep lustre of glossy Guards Red bodywork, they furnish this modern motoring icon with classic 911 looks, but don't be fooled into thinking the attention to detail is skin deep.

The car first came to Ninemeister's attention a few years ago, when the firm hosted an open evening at its then new premises. The Turbo's owner was in attendance and liked what he saw. At that point in time, with the exception of its fantastic five-spokes, the car was exactly as Porsche intended. "To begin with, we were asked to take care of basic servicing and maintenance," Peter recalls. "Later, the owner's son

became interested in 911s and their potential for vastly increased performance over factory specification. He recognised his father's 997 Turbo as a prime candidate for personalisation. Before long, we were mapping out the various stages of development to transform the car into the 570bhp beast it is today."

Derestricting airflow was the first task, not only to help the turbocharged 3.8-litre lump to breathe better, but also to promote a raspier exhaust note. To that end, Kline Innovation exhaust manifolds and 200-cell sports cats were bolted into place, ensuring torque curves and peak power were improved whilst simultaneously reducing lag. Attention then turned to the car's transmission, which was treated to a CT2 RS short shifter. "The nylon bushes in the standard Turbo linkage are prone to wear," Peter explains. "Gear changes in our customer's car felt vague. It was a condition eliminated by the GT2 kit, which boasts aluminium bushes."

The biggest change to the transmission, and one which has played a vital role in changing the car's overall driving characteristics, is the installation of a Wavetrac limited-slip differential. "It's a clean sheet, state-of-the-art design engineered to be a better differential than any other when used on the road or at the track in a high-powered car when a wheel is more likely to lift despite the presence of torque biasing," stresses Peter. "Unlike limited-slipppers from other manufacturers, a Wavetrac diff provides quicker acceleration and faster cornering by driving both wheels instead of one. In other words, »

Above Guards Red is a welcome finish in a sea of silver, black and grey 997s

Facing page KW V3 coilovers drop the ride height to GT3 standards



STAR ATTRACTION

The second-gen 997 Turbo was launched at the 2009 Frankfurt Motor Show. The new model introduced a lightweight 3.8-litre flat-six equipped with VTG turbos.



Facing page Turbo interior includes red Alcantara soon to be joined by colour-coded seat centres made from the same material

it'll allow transfer of power between wheels even if an axle isn't loaded. Lift a wheel in a corner, and you'll still get power sent to the wheel on the ground. This doesn't happen with other differential designs, which make it possible to lose traction if you hit a pothole or encounter similar surface defects. With a Wavetrac unit in place, the host Porsche's road manners remain unaffected, yet performance is drastically improved." Clearly, the design of a performance part is as important to the team at Ninemeister as the effect produced by the component following installation.

BIGGER PICTURE

Peter cites the engineering-focused career path of company boss, Colin Belton, as one of the reasons Ninemeister has forged a reputation for technical excellence when it comes to the planning and execution of the 964-based restomods the Cheshire-based marque specialist has become famous for in recent years. "What people don't always realise is that the same level of consideration is applied to every Porsche entering the Ninemeister workshop," says Peter, highlighting the 997 Turbo on the pages before you as a solid example of where an owner wanted to enhance their pride and joy without the need to commission an accompanying restoration. "The suspension," he goes on to say, "is typical of this approach, where we installed KW Variant 3 coilovers after assessing their suitability for the car in terms of the driving environment it's likely to find itself in. The V3s offer comprehensive bump and rebound adjustability at the same time as affording us the opportunity to dial-in a GT3-esque ride height, thereby enhancing the car's appearance at the same time as significantly improving handling."

He's not wrong. The dampers work brilliantly alongside the Wavetrac differential and the sticky Michelin rubber wrapped around each rim to ensure this red road riot remains firmly planted to the asphalt at all times. No matter if you're throwing the car into a corner or propelling it at breakneck speed along a straight, the sure-footedness delivered by the revised chassis inspires elevated levels of confidence when exploring the additional power Ninemeister has extracted from this sublime 997. "It's how Porsche should have built it at the factory," Peter shrugs.

He's quick to dismiss the idea Ninemeister might market the parts fitted as an off-the-shelf 997 Turbo performance package. "The company is more concerned with engineering than marketing!" he laughs. "That said, we would certainly adopt the same approach to evaluating and developing a similar setup for any other customer's water-cooled 911," he adds, suggesting there's no such thing as 'one size fits all' in the Porsche world. "A wealth of options is available for us to plunder, but we'll always put the customer's requirements above our perception of what a car will benefit from most. After all, every driver is different, meaning what's good for one isn't necessarily suitable for the next."

When reluctantly handing back the keys to the car, we ask what the father and son duo encouraging the continued development of the Turbo we've just stepped out of has in mind for the next phase of the project. We're told different intercoolers and a new map are on the cards, with the possibility of a retrim bringing red Alcantara seat centres to the party as a way of carrying the exterior's retro-inspired styling into the cabin. "Mechanically and cosmetically, the 997 platform is perfect for personalisation," concludes Peter. Little wonder it became the most successful 911 of all time! ■

Below Flat-six delivers a totally unstressed 570bhp with potential for plenty more ponies





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The screenshot shows the Elephant Racing website's Suspension Builder tool. At the top, there's a navigation bar with links: Home, About, Contact, Careers, News, Tech Topics, and Shopping Cart. Below this, a secondary bar mentions 'Advanced Suspension Systems For Porsche Cars Design & Development' and 'Fast Shipping Worldwide'. A search bar and a list of model years (911, 964, 993, 996/997, 991, Boxster/Cayman, 914, 944, 356) are also present. The main heading is 'Suspension Builder'. To the right, 'ONLINE TOOLS' includes 'Suspension Navigator' and 'Suspension Builder'. The interface is divided into three steps: 1. Select Year & Model (Year: 2004, Model: 996/997/986/987), 2. Select Your Package (Street Performance 2), and 3. Review & Approve. A grid of suspension components is shown for both Front and Rear views. A 'PACKAGE CHARACTERISTICS' table is visible on the right, showing 'RIDE COMFORT' at 41 and 'PERFORMANCE' at 63. An 'Add To Cart' button is at the bottom.

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NEW 911 SPEEDSTER BEGINS PRODUCTION

Devastatingly good drop-top proves the 991 isn't retiring quietly



You'd have to be living under a rock not to witness the excitement surrounding the release of the new 911 Speedster. The concept car caused a sensation when presented at the height of Porsche's birthday celebrations last year, with the 991 R and 991 GT3 serving as the basis for development. Now, series production begins, although only 1,948 units will be manufactured, a figure paying tribute to the "No. 1" 356 roadster of 1948.

Taking centre stage is the distinctively shaped drop-top compartment lid with its double-bubble streamlined headrest cowls. Made from a single piece of carbon-fibre reinforced plastic, it's the largest and most

complex component of its type ever fitted to a Porsche road car. Two trim elements in the double-bubble make room for the standard roll-over protection system.

A weight-saving roof structure replaces the basic tonneau cover of the concept car, while shortened window frames, lowered cowl tops and smaller side windows give new Speedster is given an athletic profile, with a low roofline hinting at the design language of the 356 Speedster from 1954.

Lightweight design is evident throughout. A carbon-fibre composite bonnet – weighing two kilos less than that of the 991 GT3 – joins carbon wings borrowed from the 991 R. The front apron is a GT3 part, but the spoiler lip is a

completely new development, unlike the aerodynamically tuned rear spoiler and attractive rear apron, parts lifted from the 991 GT3 Touring.

GT3 DNA continues with the Speedster's chassis and naturally aspirated four-litre flat-six, delivering peak power of 503bhp at 8,400rpm. Maximum speed is delivered at a whopping 9,000rpm (this is a race-developed engine, after all), while maximum torque of 347lb-ft is reached at 6,250rpm. These figures work together to ensure a sprint to 62mph from a standing start in four seconds, with an attainable top speed of 192mph.

Compared to the 991 GT3 it lovingly borrows its guts from, the new Speedster



delivers an extra ten ponies thanks to a refining of fuel injector spray patterns and a modified intake. A new lightweight stainless steel exhaust system offers a weight saving of ten kilograms, too.

As you might expect, befitting its status as a true driver's car, the new Speedster is available only with a six-speed manual gearbox. The transmission's auto-blip functionality independently compensates for differences in engine speed between gears when downshifting.

The standard interior is covered in luxurious black leather, Speedster logos and a numbered identity plaque, but those with deep pockets can option Porsche's Heritage Design package, introducing

Cognac trim and gold accents to the proceedings. Choosing this cost option also decorates the lower front and wings of the Speedster in white paint, although the rest of the car remains coated in GT Silver Metallic. Race-inspired body graphics complete the package, with buyers given the opportunity to have their personal choice of two-digit number on display.

Choosing the Heritage Design package will set you back more than fifteen grand over the Speedster's starting price of £211,599. Reach down the back of the sofa for an additional £9,900 if you want your new open-top 991 to be accompanied by a matching Porsche Design wristwatch, limited to – you guessed it – 1,948 units.



ZUFFENHAUSEN FURTHERS EFFORTS TO BECOMING CARBON NEUTRAL FACILITY

In a positive attempt to enhance Porsche's commitment to reducing carbon dioxide output by closing the gap to carbon-neutral production of the fully electric Taycan, the company has established two new cogeneration plants at its famous manufacturing facility in Zuffenhausen. The heat and power units, each with an output of around two megawatts, run exclusively on biogas and residual products derived from organic waste.

In contrast to conventional power plants, cogeneration plants are used to generate heat and power in parallel. They don't emit heat as part of typical energy production, instead retaining the heat for, erm, heating. This efficiency is maximised by operating the plants close to an area where heat is constantly required, such as a paint shop, an immersion bath or a drying area. Additionally, approximately ninety percent of the heat generated by these new installations will be used to warm and provide hot water to the Zuffenhausen site's office and production buildings, where almost 12,000 employees work.

The two new cogeneration plants will complement the advanced heat and power generation solution already in place at Zuffenhausen, where Porsche has made use of two natural gas plants. These plants will soon be switched to biogas. Additionally, Porsche is on track to use bioenergy from Stuttgart's organic waste plant as soon as the city's ruling body commissions the facility within the next eighteen months.

Taycan will be launched at the end of 2019. "Our objective is not merely to produce a carbon-neutral, zero-emissions car, but rather to ensure we do not leave any environmental footprint at all," confirmed Albrecht Reimold, Member of the Executive Board for Production and Logistics at Porsche. "Our new cogeneration plants are an important step in the right direction," he added.





CAYENNE S COUPE DRIVES AHEAD

New powerful variant of the sleek five-door goes on sale

Last month, we revealed details of the new Cayenne Coupe, a sporty derivative of Porsche's super-popular SUV. Complete with its sloping roof and a choice of turbocharged three-litre V6 or twin-turbocharged four-litre V8 engines, the model's first UK customers are due to take delivery of their force-fed utility vehicles around the time this issue of *GT Porsche* reaches newsstands. But wait! There's already a new addition to the Cayenne Coupe line-up for us to tell you about.

Slotting in between the standard Coupe and the Turbo, the Cayenne S Coupe is powered by a 2.9-litre V6 featuring a duo of turbochargers boosting power to 434bhp with torque of 406lb-ft to guarantee rapid

acceleration from rest to 62mph in just five seconds with the standard Sport Chrono package working its magic. This figure drops to 4.9 seconds with an optional lightweight sports package. Top speed is 163mph.

The turbos have been arranged inside the V between cylinder banks, resulting in an engine with compact dimensions, allowing it to be mounted lower than a conventional layout will permit. This lowers the centre of gravity and improves lateral dynamics. The shortened exhaust paths between combustion chambers and the turbos produce improved responsiveness and faster build-up of power, while the exhaust manifold has been integrated into the cylinder head, reducing weight and ensuring the manifold pipework is surrounded

by coolant, ensuring highly efficient combustion, particularly when the Cayenne S Coupe is operating under full load.

This new offering from Porsche makes full use of the dynamic proportions and design elements specific to the Cayenne Coupe range, including that sweeping roof, shallower A-pillars, revised windscreen dimensions, newly designed rear doors, more muscular wings and an adaptive rear spoiler extending by 135mm at speeds over 56mph. Twenty-inch alloys, Sport Chrono, Park Assist and Porsche Active Suspension Management (PASM) round out the package, which is yours from £73,658, almost thirty grand less than the full-fat Turbo. An appealing prospect, and one early buyers will enjoy exploring when the first UK deliveries arrive later in the year.



CLASSIC MANUALS REPRINTED

Many of you in possession of an older Porsche will be pleased to hear Porsche Classic has added more than seven hundred original driver's manuals to its portfolio of reproduction parts, ensuring owners of models as old as the 356 right the way through to the 996 will be able to obtain replacement a handbook to accompany their car. Whether the original book has been lost, damaged or is simply worn through years of enthusiastic thumbing, these newly printed manuals include warranty and maintenance booklets, vehicle servicing paperwork, audio guides, general care instructions and vehicle wallets true to the originals. Various languages are catered for.





Porsche Classic Oil Service.

Caring for the heart of your classic 911.

The Porsche Classic Oil Service for air-cooled 911 models, including Porsche Classic Motoroil and red oil-filter*, is now available at participating Porsche Centres. Prices start from £309** for 911 models (up to 2.7i G model).

For more information visit www.porsche.co.uk/classicoilservice

*Porsche Classic 'red' oil-filter not applicable to 993 generation models. **Participating Centres only. Fixed price Classic Oil Service tariffs may be withdrawn or varied at any time. Oil Service includes Classic Motoroil and oil-filter and associated parts. Excludes any additional items and checks.



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PORSCHE PRODUCTS

Each month, we present exciting parts and services designed to benefit you and your four-wheeled friend. From audio to engine upgrades, we've got you covered.

RED HOT PRODUCT

▲ **PIE PERFORMANCE 986/987/996/997 UNDERDRIVE CRANKSHAFT PULLEY**

An underdrive engine pulley is a performance enhancing part designed to increase horsepower and torque by reducing drag caused by belt-driven ancillaries. It's a part ideally suited to the engine of a sports car likely to experience frequent and prolonged periods of high rpm. The appointment of an underdrive pulley prevents engine accessories from being subjected to excessive load, a condition which may cause regrettable damage.

Sudbury-based independent Porsche specialist, PIE Performance, has developed its own underdrive crankshaft pulley kit for the first-gen Boxster. In addition to 2.5, 2.7 and 3.2-litre flat-sixes spanning years 1997-2004, the kit is also suitable for the 987 Boxster (2.7, 3.2 and 3.4-litre engines covering years 2005-2007) and the 996 and 997 generation 911 (3.4 and 3.6-litre engines covering years 1999-2005).

Manufactured in the UK, PIE's offering comprises an anodised underdrive crankshaft pulley, a new bolt and an updated auxiliary drive belt (air-conditioning or non-aircon models are catered for). Reducing resistance from engine accessory components, the kit allows you to retain use of your car's standard power steering pump cooler. The product isn't suitable for 3.8-litre S or Turbo models.

Professional installation is recommended, although the kit comes with full fitting instructions for the competent home mechanic. Choose between black, gold or red finishes for the pulley and order direct from the PIE Performance website. Alternatively, give the firm's friendly team a tinkle on the number below.

Price: £174

pieperformance.co.uk or call 01787 249924



▲ BLUE PRINT OIL FILTERS

An oil filter protects your car's engine by keeping lubricant free of dirt and contamination during a prescribed service life. Microscopic wear particles, dust and soot from combustion must be filtered efficiently to prevent degradation to bearing surfaces, damage which can easily lead to increased engine wear. Blue Print supplies oil filters with premium quality filtration media boasting the efficiency and capacity to protect between extended service intervals. With over 2,300 part numbers in its family of filtration solutions, the company offers one of the most extensive ranges available in the aftermarket. Visit the website of Blue Print's parent company, Bilstein Group, for a Porsche application list.

Price Varies

bilsteingroup.com



▲ PIONEER SLIMLINE SUBWOOFER AND DRIVER

If you're working within the confines of a cramped car cabin, Pioneer's TS-D10LB slimline subwoofer is the perfect solution when trying to achieve awesome audio. This surprisingly compact, ten-inch sub box features a 2ohm driver drawing up to 350w RMS, providing huge bottom-end welly in a small space. Frequency drops down to a low 20hz and extends all the way up to 3.4khz, the kind of punch your Porsche's passengers will be convinced is coming from a much bigger sub. Capable of squeezing into gaps offerings from rival in-car audio parts makers simply can't entertain, this well-priced Pioneer product is perfect for music-loving owners of old-school sports cars.

Price £209.99

pioneer-car.eu



▲ WEBCON TUNER DRILL SET

Leading fuel injection, ignition and carburettor specialist, Webcon, has announced the successor to its immensely popular Jet Drill tuning drill set. Presenting the Webcon Tuner Drill Set, featuring twenty-two precision drills, each increasing in size by 0.1mm from 0.6mm to 3.2mm. Five of each drill is supplied, as is a double-ended pin vice. Supplied in a handy, twin-latched plastic carry case, this low-price product is a fantastic addition to the toolbox of any DIY or professional Porsche tuner.

Price £37.95

webcon.co.uk or call 01932 787100

▼ YUASA YBX5000 BATTERY RANGE

Designed for standard or modified vehicles with factory ignition systems, Yuasa's YBX5000 performance battery range features up to 35% increased cranking power over a standard battery. A state of charge indicator (SOCi) provides a quick and easy health check, while the integrated carrying handle and flame arrestor ensure easy of transportation and peace of mind through enhanced safety. Smaller and lighter than your car's OEM battery, Yuasa's offering features a sealed tip/tilt double lid and calcium plates, adding to already enviable specification. The YBX5000 range caters for all Porsches, including air-cooled and transaxle models. Visit yuasa.co.uk and use the firm's handy vehicle finder to discover which battery and upgrade options are available for you to take advantage of.

Price From £81.95

yuasa.co.uk or call 01793 833555





MEGUIAR'S QUIK SCRATCH ERASER KIT

Working by hand to remove swirls, scuffs and scratches takes time and energy, but this comprehensive kit from Meguiar's supplies you with everything you need to eliminate light paint defects using the minimum of effort. With the assistance of a standard household drill, you'll be saying *sayonara* to the annoying marks preventing your Porsche's paintwork from looking its best. The kit includes a four-inch scratch eraser, a ScratchX 2.0 blemish remover (a compound featuring microscopic abrasives to enhance depth of shine without harming paintwork) and a premium microfibre towel. This unique combination of products allows you to safely remove imperfections in paint, leaving a brilliant shine where once all you could see was unsightly disfigurement.

Price £23

meguiars.co.uk or visit Halfords

YOKOHAMA ADVAN NEOVA AD08RS SEMI-SLICK TYRE

A re-engineered version of Yokohama's immensely successful Neova AD08R semi-slick tyre, the AD08RS has been fine-tuned to meet strict European ECE R117-02 S2WR2 road regulations, rules promoting environmentally sound performance. Whilst keeping the tread pattern that earned the AD08R a strong reputation for excellent control in dry and wet conditions, not to mention superior resistance to abrasion, the AD08RS uses a new compound, further reducing rolling resistance while achieving excellent handling and delivering lower fuel consumption. A range of sizes from 15-inches to 19-inches with a high number of profiles is due to be rolled out as the year progresses.

Price Varies

yokohama.co.uk or call 01908 625625



911 RSR MOVIE

This unique film takes a look at the iconic 911 2.8 and 3.0 RSR in detail. The cars are captured in action at Porsche's own handling circuit at Silverstone, as well as further afield at the Nürburgring. The movie's producers get up close and personal with each model, exploring their similarities and differences, while insight into what it's like to race these legendary motorsport machines comes from famous Porsche pilots, Derek Bell and John Fitzpatrick. England's own 'Mr Porsche', Nick Faure, is featured alongside his Harley Davidson-sponsored 3.0 RSR, while in-depth technical analysis makes this a must-see movie for anyone passionate about powerful classic Porsches.

Price: Rent from £4, DVD £19, Blu-ray £24
911racers.com



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Designed to replace any existing round headlight measuring seven inches in diameter (such as those fitted to classic 911s), these fantastic LED headlights from Wipac follow an exclusive design and manufacturing process to ensure unrivalled illumination. Fully E-marked and road legal in the United Kingdom and mainland Europe, this intense white lighting upgrade is sold as a package comprising a pair of headlights with halo sidelights and a choice of black or chrome bodies.

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wipac-led.com or call 01280 825825



RICHBROOK DIGITAL TYRE PRESSURE AND TREAD DEPTH READING GAUGE

This digital tyre pressure and tread depth gauge from automotive accessories specialist, Richbrook, allows you to quickly and easily check your car's, erm, tyre pressure and tread depth. Measuring just 60x40mm, the compact tool is conveniently portable and made from sturdy materials with an easy-to-read multi-character LCD display screen. Pressure is registered in psi, bar kPa and kg/cm², while an air bleed function allows for precise readings. The gauge comes complete with its own storage pouch and is offered at a pleasing price point.

Price: £19.95

richbrook.co.uk or call 01328 862387



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Covering every generation and body style of 356, this 64-page pocket-sized buyer's guide authored by model expert, Brett Johnson, provides guidance on not only what to look for when eyeing up your prospective purchase, but also key considerations when determining which 356 is right for you. Johnson's many years of 356 ownership and more than a decade working in the Porsche parts business provide valuable insight into the rights and wrongs associated with the model, ensuring you spot bad cars quickly by way of a handy points scoring system. It's a feature of all buying guides in this massively popular series from automotive publisher, Veloce. 150k copies sold worldwide and a range of Porsches covered (including air-cooled

and transaxle speed machines from Stuttgart) means you can be confident a Veloce buying guide is the equivalent of having a knowledgeable marque specialist by your side, enabling you to buy the right car at the time for the right price. Good luck!

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Arctic Silver • Black Leather Bucket Seats • Manual Gearbox • Rear Roll Cage • Porsche Ceramic Composite Brakes • 49,352 miles • 2002 (02)

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911 Turbo S (997)

Carrera White • Black Leather Sport Seats • PDK Gearbox • Porsche Ceramic Composite Brakes • 19,524 miles • 2011 (11)

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911 Carrera Sport Targa

Grand Prix White • Dark Blue Leather Seats • Manual Gearbox • 16" Fuchs Wheels • Fully Electric Seats 22,373 miles • 1988 (F)

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911 GT3 (996)

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911 Carrera 4 GTS (997)

Guards Red • Black Half Leather Sport Seats • PDK Gearbox • 19" Centre Lock Wheels • 28,593 miles 2011 (61)

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911 Turbo (997) GEN 1.5

GT Silver • Cocoa Leather Bucket Seats • Manual Gearbox • 19" Fuchs Wheels • Touchscreen Satellite Navigation • 51,436 miles • 2009 (58)

£64,995



911 Turbo (996)

Seal Grey • Black Leather Sport Seats • Manual Gearbox • Satellite Navigation • 18" Turbo Wheels 56,838 miles • 2002 (02)

£44,995



Boxster (981)

Basalt Black • Black Leather Sport Seats • PDK Gearbox • 20" Carrera S II Wheels • Touchscreen Satellite Navigation • 15,450 miles • 2012 (62)

£31,995



911 Carrera 2 S (997)

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X5M50D/X6M50D » 450BHP
M135i/M235i » 410+BHP
i8 » 415BHP
120i/220i/320i/420i » 275+BHP
116D/216D/316D » 160BHP
118D/218D/318D » 225BHP
120D/220D/320D/420D » 240BHP
328i/428i » 295BHP
335i/435i » 410+BHP
330D/430D/530D/730D » 360BHP
335D/435D/535D » 395+BHP
550i/650i » 555+BHP (+DE-LIMIT)
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55 AMG KOMPRESSOR » 600+BHP
(+DE-LIMIT & SUSPENSION LOWERING)
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SL65 AMG » 690BHP (+DE-LIMIT)
SL65 BLACK » 720BHP (+DE-LIMIT)
SLK 55 AMG » 420BHP
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220 CDI (ALL MODELS) » 230BHP
250 CDI (ALL MODELS) » 260BHP
C300 HYBRID » 285BHP
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MCLAREN 650S » 720 BHP
MCLAREN 675LT » 750BHP
MCLAREN 570/S » 680+BHP
AVENTADOR » 750+BHP
HURACAN LP610 » 650BHP
GALLARDO LP560 » 600+BHP
BENTLEY 4.0 T V8 » 700BHP
BENTLEY GT/F-SPUR » 680BHP
GT SPEED / SUPERSPORT » 690+BHP
BENTAYGA W12 » 700+BHP
MASERATI GHIBLI 3.0S PETROL » 470 BHP
MASERATI GHIBLI 3.0 DIESEL » 400 BHP
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BITTEN BY THE BUG

A test drive in a 1973 Carrera RS 2.7 replica was all it took for Michelle Hambly-Grobler to ditch American V8s in favour of Porsche perfection

Words & Photography **Sudhir Matai**

The South African port city of Cape Town isn't necessarily the first location you'd think of if asked to imagine where there might be a hidden treasure trove of Porsches to marvel at, but the southwest coastal spot known locally as Mother City is precisely where Michelle Hambly-Grobler stores her stash of Stuttgart's finest. "I used to live in an upmarket neighbourhood on the edge of Cape Town, but parking was a problem," she tells us, following our arrival at the factory she's recently transformed into luxurious living space. "My cars were scattered all over the place. In the end, my husband and I decided to convert this old industrial site into a home which can accommodate our family, my cars and my mountain of Porsche memorabilia."

FAMILY AFFAIR

Mirror-finish white tiles cover the ground floor, but before we get to explore, Michelle leads us up a flight of stairs to her office, where we're treated to coffee and a history lesson. "My love affair with cars stretches back to my formative years," she recalls. "My grandfather owned a Ford garage in Ceres, a town located a couple of hours outside Cape Town. My uncle used to race a Renault Gordini at a local oval circuit. I have fond memories of my mother piling us all into her Zephyr late on Friday nights, when we'd set off to watch Uncle Les do his thing at the track. As you can tell, as a youngster, I was surrounded by cars. It's hardly surprising I'm obsessed with them today!" »



Our eyes wander around the room to witness dozen upon dozen of die-casts, most of them small-scale Porsches. Michelle points to a red toy car over her shoulder. It's a tiny Volkswagen Beetle. "When I started life at university," she reminisces, "I bought myself a Karmann Ghia. It looked great, but it cost a fortune to fill up with fuel and it leaked oil wherever I went. My father suspected a crack in the engine block was to blame for leaking lubricant." An inspection by a marque specialist confirmed the worst. "I swiftly got rid of the car and bought a red Beetle!" she laughs.

CHANGE OF DIRECTION

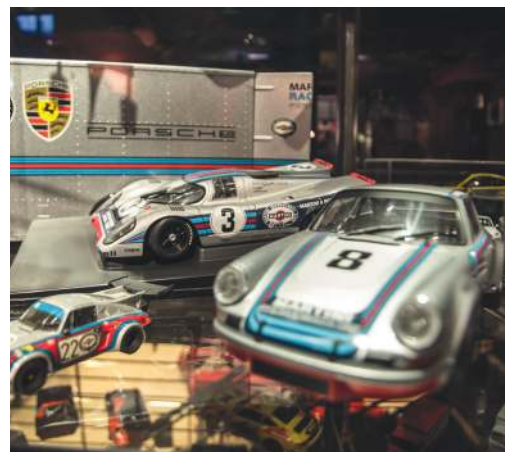
Marriage followed her higher education, as did the arrival of six children. Ferrying a large troop of youngsters from place to place meant a motoring life restricted to time in charge of SUVs and people carriers, but the yearning to own a special sports car stayed strong. "Sixteen years ago, when my oldest kids were getting ready to finish school, I bought myself a 1968 Ford Mustang," she beams. A 1958 Corvette soon joined the party. "It was a surprise gift from my husband." From that moment, Michelle embraced classic car ownership like never

SHE EMBRACED CLASSIC CAR OWNERSHIP LIKE NEVER BEFORE

before, yet it was an unexpected test drive in an air-cooled Porsche that set her on the path she currently treads.

She recalls the moment clearly. "It was 2012, and I was afforded the opportunity to spend time behind the wheel of a friend's 1973 Carrera RS 2.7 replica. I drove the car for close to fifty miles and was completely bowled over by the experience. I immediately wondered why I was wasting my time with American V8s when I could be enjoying Porsche ownership. I ditched the Mustang, and before I knew it, I was venturing along the slippery slope of collecting air-cooled classics."

We walk back downstairs to explore her fantastic fleet, which includes a pristine Porsche-Diesel Junior tractor recently restored in Germany. It feels as though we're wandering through a well-kept museum, much less an active garage. Signed posters, technical





Facing page Users of social media may know Michelle by her Instagram handle, PorscheGirlCT

Top left Michelle's first Neunelfer was the spotless white 911T seen here parked next to her Irish Green 911S



Bottom left This Porsche-obsessed collector aims to own one of every 911 Turbo

Left Red 964 is the only non-manual car in Michelle's automotive vault





Above Michelle's workstation is littered with historic racing paraphernalia and, er, GT Porsche magazine!



Right Newer cars recently acquired include a gorgeous 993 Turbo, a 997 GT3 RS and a Cayman GT4



Facing page Porsche-Diesel Junior tractor was recently restored in Germany before finding its way into Michelle's fleet

Overleaf Yes, that really is a tattoo depicting a 356 coupe and Porsche script



drawings, race car parts, Porsche apparel and countless books adorn the walls of Michelle's personal playground. She draws our attention to the first 911 she bought, a beautiful white 911T. Riding on Fuchs and spotless in every way, the car was quickly joined by a concours 911S. "I couldn't resist!" she giggles. "The Irish Green paintwork matched with Houndstooth fabric blew me away. Unfortunately, I had to sell four other cars in order to meet the seller's asking price, but I have no regrets. I love my little green 911!"

GREAT PRETENDER

The two early 911s are joined by a 912, a 356 B, the aforementioned tractor and a bright red 356 Speedster replica built using as many genuine Porsche parts as possible. It's a car that has gone on to feature in many television commercials and print advertisements, with even die-hard Porscheophiles struggling to tell the lookalike apart from the real deal.

More modern metal catches our eye on the other side of Michelle's stunning storage facility; a 964 imported from America and wearing Carrera Cup livery sits alongside a right-hand drive Guards Red 930. There's a 993 Turbo, plus even

THE ONLY NON-MANUAL CAR IN HER COLLECTION IS A TIPTRONIC 964

more recent samples from the Porsche product portfolio, including a 997 GT3 RS and a Cayman GT4. The far end of the garage is inhabited by the only non-manual car in the collection: a 964 loaded with Tiptronic transmission.

Two G-Series 911s (an SC and Targa) and a rare Type 34 'Razor Edge' Karmann Ghia in immaculate condition join the newer cars in the Hambly-Grobler garage, as does as black 944 race car. The track-focused transaxle is Michelle's latest acquisition and one she intends to use as a way of cutting her teeth in local motorsport competitions. Don't think an interest in racing means she's finished adding to her amazing cluster of Porsches, though. "My plan is to amass a complete set of 911 Turbos," she smiles, her eyes lighting up at the thought of expanding her compilation of modern classics. "996 and 997 Turbos are next on my list. I'm also considering »

CLEARLY, HER PASSION FOR PORSCHE KNOWS NO BOUNDS

the purchase of a 911E to join my 911T and 911S. 996 and 991 GT3s would be great, of course. Oh, and since I already own a lovely 356 B, I guess a 356 A and a 356 C would be a good idea?! Then there's my desire to further explore the transaxle family of Porsches. Owning a 944 Turbo and a 928 S4 would be great fun. The list goes on!"

Considering her current quota of Porsches was amassed in a few short years, we have no doubt some of the models mentioned will be arriving at Michelle's place of residence in the near future. There is, however, a question we're dying to know the answer to: what one Porsche would encourage her to part with all the cars she now owns? "That's easy to answer," she smiles "A Carrera GT! On second thoughts, maybe a 959 Komfort. Can I choose a 917? How about..." Clearly, her passion for Porsche knows no bounds!








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NEEL JANI

Porsche's popular Le Mans champ experiences the thrills and spills of racing a V8-powered Rover SD1

My father is like a walking encyclopedia of classic cars, so you can imagine how pleased he was when I invited him to join me at the recent Goodwood Members' Meeting. I'd visited the famous Sussex racing venue when I was part of the team responsible for showcasing the 919 Evo at last year's highly anticipated Festival of Speed. My return stay in Chichester, however, was marked by a stint behind the wheel of an altogether different type of racing machine, namely the 3.5-litre Patrick Motorsport Rover SD1 V8 I drove in the event's respected Gerry Marshall Trophy.

Compared to what I'm used to, the racing cockpit I found myself in was totally alien. For a start, the SD1 is a right-hand drive car with a stick shift on the left. Then there were the brakes to contend with. I was dealing with drums at the rear, as well as heavy shifting and wallowy suspension. The Rover's owner, Robert Brooks, and my teammate, James Wood, gave me a useful introduction to the car before the race, but I remember having to tell myself, "you're not in the 919 Evo now!"

My 2016 Le Mans teammate and fellow Porsche factory driver, Romain Dumas, was participating in the same competition from within the confines of a Ford Mustang Boss 302. Both of us qualified well, with Romain positioning the Pony car in pole. James managed to nudge the big five-door into P3. There was no staggered grid, just wild historic racing machines lined up next to one another on the start line, exactly how they would have appeared in their heyday.

Enthusiastic spectators saw me quickly fire the Rover into the lead. They also saw me come unstuck at the first corner! Muscle memory encouraged me to forget I wasn't in the 919. I hit the brakes late, a move resulting in the SD1 going wide and hitting the dirt before falling back to P5. My early 'off' was a wake-up call, and one which stuck with me for the rest of the race.

Putting what I'd just learned into practice, I switched my focus from entry

speed to exit speed and pressed on with an ambition to leapfrog every car ahead. James was just as determined when he took the controls. Amazingly, despite my little 'moment' at the start, the bright blue barge crossed the finish line in second place. The story changed shortly after the race, when we heard the winner had been disqualified for a technical infringement. The Rover was moved up to P1! Lady Luck had helped me to win my first ever historic racing event.

Away from the track, I was amazed at the massive volume of vintage race cars on display. I'm lucky enough to be familiar with the workings of the aforementioned 919 Evo, the Porsche I used to break the lap record at Spa last year, but seeing race cars of all ages laid out next to one another was like witnessing automotive evolution. It was clear to see not only how motorsport technology has progressed over the years, but also how the approach to sports car design has changed. For example, despite F1 and rally legend, Vic Elford, telling me the 917K is a comfortable vehicle to sit in, it seems clear today's high-end race cars are designed and built around the driver, whereas the guy behind the wheel of retro racing metal was required to squeeze into whatever space was left for him.

Don't get me wrong. The 917K is a great car to be in charge of. Its transmission is smooth, the balance of power and traction is divine. Even so, I'm happy not to have been required to drive the car on the limit, especially at night, when dim illumination from the model's nose doesn't exactly inspire confidence. Needless to say, I have nothing but respect for the Porsche works heroes who took on the task of piloting the twelve-cylinder sports prototype in period.

I was thrilled to accept the Duke of Richmond's invitation to attend the 77th Members' Meeting. I'm looking forward to returning to Goodwood in the near future. I've been exposed to a new world of racing in classic cars. It's a pursuit outside my comfort zone, but one that's proved hugely rewarding and made me a better driver. ■



Neel Jani is best-known for winning the 2016 24 Hours of Le Mans as a Porsche works pilot. He recently joined the manufacturer's Formula E driver line-up and is a former World Endurance Championship, A1 Grand Prix and F2 title winner.

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GARY MAVERS

The celebrated star of stage and screen recalls the Porsche responsible for his love of classic cars

A couple of years ago, while working at the Oxford Playhouse, I had a chance encounter with a charming elderly gentleman. "A very long time ago," he started in a broken German accent, "I was involved in the design and development of these cars, along with Ferraris and the first Lamborghini passenger vehicle." He was admiring my Aston Martin. Over the course of the next half hour, the fascinating conversation outlined how my new friend still had close ties to the Lamborghini family, the fact he'd helped develop many Mercedes race cars and how he'd personally known and worked for Enzo Ferrari. The old boy concluded by telling me the Porsche engineering he was exposed to during his long career was far superior to output from other sports car manufacturers.

The talk took me back to being a 22-year-old budding actor, fresh out of RADA, debuting at my hometown theatre in Liverpool and my first encounter with a sports machine from Stuttgart, an experience which kick-started my lifelong love affair with classic cars. I was visiting the production's musical director at his home near Sefton Park. He showed me what was essentially an abandoned 356 B. The car had been ditched by a pair of Italian racers who'd decided to drive around the world, but after blowing up the Porsche's engine outside my host's house – and without the funds to commission the necessary repair work – they'd traded the car with him in exchange for flights back to Italy. With this romantic story ringing in my head, not to mention the sight of the forlorn four-wheeled fallen hero, complete with its sticker-covered white paintwork and huge numbers painted on the doors, I made it clear I wanted to buy the car. Sadly, my pleas fell on deaf ears.

A short while later, I purchased a Mercedes W113 230 SL 'Pagoda' and began travelling the long, lonely road of the home restorer, learning my craft the hard way. Since that time, I've owned and enjoyed many modern Porsches, but a few years ago, I came across an impotent 924 that had been left

in a garage for close to three decades. I'd been contacted by a retired doctor. He lived locally and was offering a classic Alfa Romeo Spider, albeit one in need of total restoration. After a five-minute walk from my house, I was shown into a rundown garage sporting a large hole in its roof. There, next to the Alfa and hiding beneath a pile of dust was the sorry-looking Porsche. "What's the story?" I asked. "I bought both cars new, but I didn't get around to piling on the miles. The years have rolled by, and now I need the space," came the reply.

Stepping into the 924's cabin was like opening a time capsule from the 1980s. The odometer read just 14,000 miles. Unfortunately, though the interior looked like new, touching the pinstriped velour covering each seat was like peeling the skin off a rice pudding. Even so, my status as a sucker for low-mileage originality meant the poorly Porsche had to be rescued!

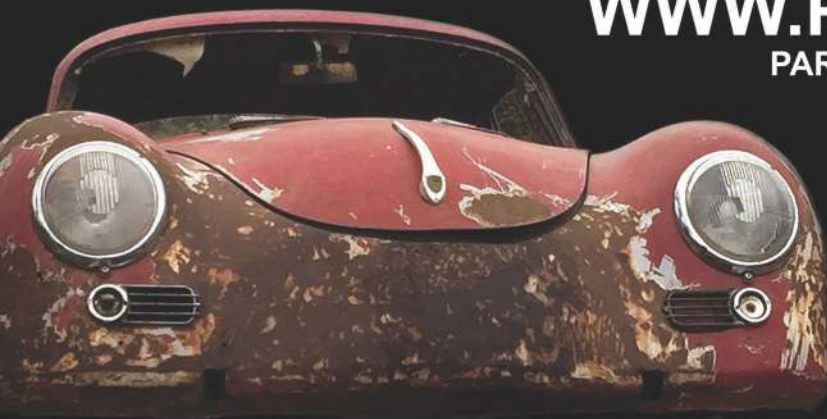
I set about the arduous task of sourcing the correct seat material from Paris, recovered the affected upholstery, fitted a new fuel pump, cleaned out the tank and installed a new exhaust. In truth, the majority of the work I faced concerned the elimination of stubborn dust and cobwebs. Thankfully, after the toil was complete, the refreshed car burst into life, the engine still wearing its original factory stickers and sounding every bit as good as the day it rolled off the production line.

Over the years, I've been lucky enough to own a selection of fantastic cars. I've searched high and low for many models, but as was the case with my 924, some have managed to find me. My memory of the 356 B, however, has stayed strong. You see, I really wanted that Porsche. I was captivated by its racing livery, its amazing story and its blown-up engine. I recently bumped into the same musical director who introduced me to the car all those years ago. Our chat quickly moved on the subject of his 356. "That old thing? I gave it away a couple of years ago," he said. "It was no use to me and I'd never have found time to restore it." Alas, the one that got away! ■



Gary Mavers has commanded the stage in many popular plays and has starred in leading roles for various high-profile television shows, including *Peak Practice*, *Emmerdale* and *Casualty*. Away from acting, he's a vintage vehicle restorer with a strong love for Porsche and Mercedes sports cars.

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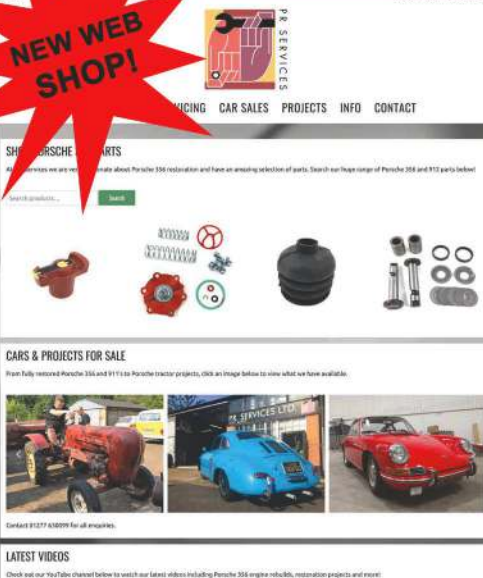
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PHILIP RABY

How does seat time in American muscle and a Japanese joy toy compare to piloting a Porsche?

After more years than I care to remember writing about and being immersed in the world of Porsches, in terms of driving pleasure, I reckon they're impossible to beat. My business, Philip Raby Specialist Cars, is primarily an independent Porsche dealer, but from time to time, models from other marques appear on the company's stock list. I enjoy trying these cars out, to see how they compare.

Recently, we prepared an Audi RS5 for sale. This was an impressive machine, with a twin-turbo V6 engine producing a hefty 444bhp and 600Nm of torque, equating to an incredible 0-62mph time of 3.9 seconds. That's up there with a Porsche 911 Turbo. The RS5 had a sumptuous interior, all pleated leather and knurled aluminium, plus lots of tech to play with, including a fully electronic dashboard. The engine sounded purposeful and there was no doubting the car's performance. It would have been an ideal long-distance cruiser but, as a driver's car, it left me cold. To my mind, it seemed a tad sterile and uninvolving.

When we sold the RS5, we took a 2016 Ford Mustang V8 5.0 in part-exchange and, I must confess, I was as excited as a child on Christmas morning. There was something about the big muscle car that appealed to the kid in me. And, no wonder. It looked like a huge Hot Wheels toy; bright red, a massively long bonnet and big black alloys. The interior had some nice touches, too, including a speedometer labelled "ground speed". All paled into insignificance when I pressed the Start button and that five-litre V8 lump came to life. What a wonderful noise! A deep, purposeful burble that sounded good, even at idle. I couldn't avoid grinning every time I blipped the throttle!

To drive, though, the Mustang just didn't do it for me. Sure, it was fast and it handled surprisingly well (much better than you'd expect from a yank tank), but there are only so many times you can drop down a gear, floor the throttle and grin inanely at the engine noise. It just seemed to be, and apologies for the pun, a one-trick pony.

I went straight from the Mustang to a 993. Nothing fancy, just a nice, ordinary non-VarioRam Carrera. I was so happy to be back in a 911. It just felt right. Compact, lively, communicative, solid and an absolute joy to drive, with enough power and torque to have fun, plus that lovely flat-six wail at high revs. Nothing compares to a 911.

Well, maybe one thing does, and that's the Mazda MX-5 we've just sold. It was a top-of-the-range 2016 Sport Recaro, the current-shape car which, amazingly, is the same size and weight as the original MX-5 of thirty years ago. That in itself is remarkable in these days of cars getting bigger and more bloated (992, anyone?). The little MX-5 wasn't particularly powerful, but it was stupidly good fun to drive, with a back end you could slide out at a moment's notice with the confidence of knowing it was very controllable. Add an engine you could actually see and a lovely snicky gearchange, and you had a real throwback to old British sports cars, albeit with everything working properly and mod-cons such as a DAB radio, sat-nav and parking sensors. The car wasn't overly high-tech, though – you opened and closed the roof with one hand. There were no fancy electrics to go wrong. It was, in many ways, the perfect sports car.

If I'm honest, I fell in love with the little Mazda. It was all the car I needed, delivering an ideal mix of fun, practicality and affordability. I remember some years ago, there were rumours of Porsche producing a smaller sports car to slot-in below the Boxster (which has grown ever-larger). It's a shame it never happened, allowing Mazda (and the rather more retro-looking Fiat 124 Spider, which is based on the MX-5 and, weirdly, has exactly the same interior) to come along and fill that niche.

I was sorry to bid farewell to the Japanese roadster when it went off to its new owner, but with a showroom full of Porsches again, I know where my loyalties lie. A drive in a 996 Turbo was enough to remind me why I've spent my career with Stuttgart-crested sports cars. Now, excuse me while I evaluate the Cayman which has just arrived...



Philip Raby has forged an enviable reputation as a trusted specialist Porsche dealer and marque consultant. With many years spent in the magazine business, he wrote hundreds of articles and several books about Porsches before establishing Philip Raby Specialist Cars, selling Porsches and other prestige vehicles.

Twitter @rabyporsche
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WE TOOK A MUSTANG V8 IN PART-EXCHANGE AND, I MUST CONFESS, I WAS AS EXCITED AS A CHILD AT CHRISTMAS

GTSTREET RS

STREET ART

Producing 759bhp, 679lb-ft torque and a top speed limited to 211mph, TechArt's recently unveiled GTstreet RS is a force to be reckoned with, either at the track or on the road

Words **Guido Komp** Photography **Tobias Kempe**



STREET SURVIVORS

Since the presentation of TechArt's first GTstreet 911 (a 996-based bruiser) in 2001, the brand has gone from strength to strength, not only as a producer of complete vehicles, but as a supplier of accessories for all water-cooled Neunelfers.



With its hulk-like hue, more than 700bhp and lashings of lightweight weave, TechArt's 991 GTstreet R caused a sensation after appearing in the February 2017 edition of *GT*

Porsche. A Clubsport-inspired interior featuring a colour-coded roll bar, six-point safety harnesses and bespoke upholstery formed from soft leather and Alcantara announced the model's status as a weapon capable of putting in super-quick lap times whilst keeping the driver in the lap of luxury. Power, comfort and aggressive style were offered in abundance, yet the Leonberg-based Porsche tuner was already planning to develop the GTstreet R far beyond what we were presented with at the time of the model's release into the wild.

TechArt was established in 1987 by company founder, Thomas Behringer. Along with RUF and Gemballa, TechArt is recognised in as an independent manufacturer in Germany. The firm's GTstreet brand has earned Behringer's boys a dedicated fanbase, not least of all thanks to the big-winged, limited production 911-based beasts they've built to challenge long held circuit records all over Europe. Indeed, exactly a decade ago, with the assistance of advanced aerodynamics and turbochargers turned up to eleven, TechArt's 997 GTstreet RS completed a lap of the 2.3-mile Sachsenring circuit in less than ninety-two seconds.

THE FLAT-SIX GETS NEWLY DEVELOPED VARIABLE GEOMETRY SNAIL-SHAPED BHP BOOSTERS

You'll note the addition of an S on the bahnstormer's badging. Unsurprisingly, the same emotive letter has been slapped onto the arse end the 991 GTstreet R. This green giant could already dash to 62mph from a standing start in 2.7 seconds, so what's new? Well, quite a bit, as we discovered at sunny Hockenheim shortly after the 991 GTstreet RS's unveiling at the Geneva Motor Show.

The body decoration and aero kit components designed and fitted to TechArt's creations are cleanly crafted and adapted, as though they came straight from the Porsche production line. Those added to the 991 Turbo S-based GTstreet RS may be an acquired taste, but there's no denying their high-quality fit and finish.

The aggressive ribs of the marble-effect rear diffuser, those front bumper canards, the sweeping rear wing. They're as purposeful as they are eye-catching, a quality encouraged by randomly arranged carbon strands delivering a distinctive aesthetic. As mesmerising as they are to look at, each of the car's lightweight carbon-fibre panels adds a huge amount of carefully considered airflow control, ensuring well-balanced aerodynamics, remarkable lateral acceleration and, of course, confidence-inspiring stability and safety when travelling at rapid pace. Don't worry, we'll address the latter shortly!

The front apron features an active lower lip optimising airflow to the water and oil coolers through additional »

intakes and a central air outlet. The integrated headlights are surrounded by carbon airframes. Strategically positioned flaps provide additional downforce and help to further improve drag coefficient. There's the familiar sight of the GTstreet R's carbon air scoop on the engine lid, as well as larger air outlet ducts at the rear to encourage a more efficient flow of air through the intercoolers.

Back at the nose, we notice the serial number of the car lacquered into the carbon-fibre bonnet, a panel complete

CUSTOM COILOVERS LINKED TO PDCC ARE BEING READIED, AS IS A NOSE-LIFT SYSTEM

with NACA ducts feeding the brakes with a plentiful supply of cool air. Another striking new development is the inclusion of carbon-fibre "aero rings" at each corner. They're inserts applied between the spokes of the GTstreet RS's twenty-inch Formula IV centre-lock wheels and are intended to act much like the flat-faced rally rims of yesteryear, diminishing air turbulence and protecting the brakes from being damaged by stones or whatever else might be flicked up off the road or track.

Ah yes, the track. That's what we've landed in Baden-Württemberg for, after all. The twin-turbocharged flat-six at the rear gets newly developed variable-geometry snail-shaped bhp boosters, which, as we discover, ensure consistent power delivery from the 3.8-litre lump, all the way past the already impressive GTstreet R's output, settling at a whopping 759bhp. That's a huge leap ahead of the older model, and an achievement that makes itself known through satisfyingly loud volume produced by four centrally located titanium tailpipes.

GREEN WITH ENVY

Torque, so TechArt tells us, is 679lb-ft. The zero-to-sixty-two dash is achieved in 2.5 seconds. It's less than half a second quicker than the stock Turbo S, but the main gain comes when hammering along with your foot planted firmly to the floor; 125mph is reached in a scant 8.1 seconds. Top speed is limited to 211mph. "We need to keep it that way for public road use," says the TechArt representative responsible for introducing us to the car, seemingly oblivious to how extraordinary his statement sounds to those of us able to hear him above the rumble of the green wide monster on idle.

Less surprising is his claim the GTstreet RS will be sold in limited numbers. Very limited, in fact, with no more than ten examples slated for production. "This is the development car," he continues. "There's still much work to be done." Custom coilovers linked to Porsche Dynamic Chassis Control (PDCC) are being readied for action, as is an advanced nose-lift system.

What we don't expect to change is the interior. Alcantara, leather and the inclusion of virgin wool – fluff taken from a lamb's first shearing, when



MATERIAL WORLD

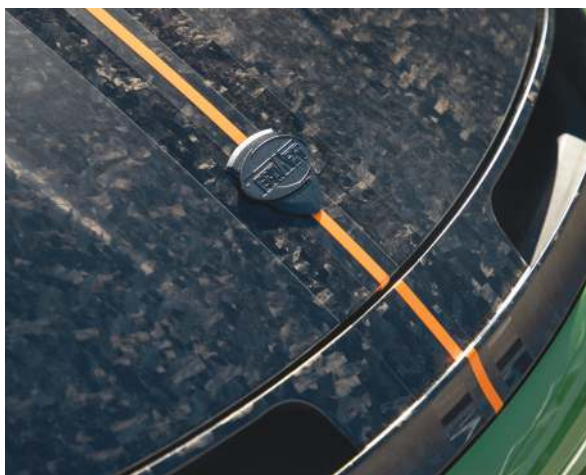
Alcantara is a microfibre material produced by combining advanced spinning techniques and chemical production processes which work with one another to create a smooth, suede-like finish. Alcantara was developed in Japan in the 1970s.





ALTERED STATE

Variable-geometry turbochargers, often referred to as VGTs, are designed to allow the effective aspect ratio (A/R) of the turbo to change in accordance with engine operating conditions. In years gone by, it was accepted that a smaller A/R would result in easier and quicker spooling of the turbine wheel, but at the cost of top-end power. Larger A/Rs, on the other hand, would produce much higher power when working at full capacity, but would be slow to spool and wouldn't allow the turbocharger to function to its performance potential when at low speed. This condition is what you'll know as 'lag'. Advances in technology mean that A/R on VGTs can be altered as the host engine works its way through the rev range, ensuring optimum turbo performance at all speeds, resulting in minimal lag, punchier output in all driving environments and increased efficiency, thereby reducing the car's emissions.



THE WINDOWS ARE MADE FROM THE SAME MATERIAL USED FOR THE SCREEN ON YOUR SMARTPHONE

the softest, finest wool produced during the sheep's lifetime is obtained – shapes the GTstreet RS's cabin. As you'd expect, no detail is left untouched. You can take advantage of complimentary handcrafted accessories to go with your new GTstreet RS, too. Fancies include a safety helmet housed in a cockpit-matching carry case, a document wallet and a travel bag. In what we think is a nice touch, each of these parts is numbered to match the identity of the GTstreet RS they're bundled with.

Top Complimentary safety helmet is numbered and colour-coded to match the accompanying 991 GTstreet RS

The rear and side windows are made from high-tensile Corning Gorilla Glass. It's the same chemically strengthened, scratch-resistant, lightweight material used for the screen on your smartphone, although the only number you're likely to be concerned with here is the price, which, our man from TechArt tells us, is likely to remain undisclosed. How does the saying go? Ah, yes. "If you have to ask..."

FRIENDS REUNITED

We complete another joyride of this mean, green, track terrorising machine. Precision typical of a modern 911 inspires confidence at every turn, but with even more hardcore steering and suspension equipment due to hit the GTstreet RS in the weeks following publication of this feature, we're looking forward to returning to Germany for a test drive of the finished build. Watch this space! ■



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PORSCHE 911 (997) "4S" 3.8 "GEN 2" PDK (11 - 2011)
Basalt black with black leather
£48,000 55,000 miles



PORSCHE 911 (997) "4S" 3.8 CAB "GEN 2" PDK (09 - 2009)
Meteor grey with black leather
£46,000 45,000 miles



PORSCHE 911 (997) "4S" 3.8 "GEN 2" PDK (59 - 2010)
Basalt black with black leather
£46,000 59,000 miles



PORSCHE 911 (997) "4S" 3.8 "GEN 2" PDK (09 - 2009)
Black with black leather
£45,000 55,000 miles



PORSCHE 911 (997) "4S" 3.8 "GEN 2" PDK (58 - 2008)
Meteor Grey with black leather
£43,000 61,000 miles



PORSCHE 911 (997) "2S" 3.8 "GEN 2" PDK (09 - 2009)
Basalt black with black leather
£42,000 47,000 miles



PORSCHE 911 (997) "2S" 3.8 "GEN 2" (60 - 2010)
Basalt black with black/grey leather
£39,000 70,000 miles



PORSCHE 911 (997) "C2" 3.6 "GEN 2" PDK (09 - 2009)
Arctic silver with ocean blue leather
£36,000 51,000 miles



PORSCHE 911 (997) TURBO CAB (58 - 2008)
Macadamia with cream leather
£65,000 25,000 miles



PORSCHE 911 (997) TURBO 3.6 (07 - 2007)
Basalt black with black leather
£56,000 53,000 miles



PORSCHE 911 (997) TURBO TIP (57 - 2007)
Basalt black with black leather
£52,000 58,000 miles



PORSCHE 911 (997) "4S" TARGA 3.8 TIP (08 - 2008)
Basalt black with black leather
£38,000 49,000 miles



PORSCHE 911 (997) "4S" CAB 3.8 TIP (08 - 2008)
Basalt black with black leather
£36,000 45,000 miles



PORSCHE 911 (997) "4S" X51 (57 - 2007)
Atlas grey with black leather
£36,000 64,000 miles



PORSCHE 911 (997) "2S" 3.8 (08 - 2008)
Midnight blue with grey leather
£35,000 52,000 miles



PORSCHE 911 (997) "4S" 3.8 (08 - 2008)
Basalt black with black leather
£34,000 63,000 miles



PORSCHE 911 (997) "4S" 3.8 (07 - 2007)
Basalt black with black leather
£34,000 62,000 miles



PORSCHE 911 (997) "4S" CAB 3.8 TIP (07 - 2007)
Basalt black with black leather
£34,000 48,000 miles



PORSCHE 911 (997) "4S" 3.8 (07 - 2007)
Basalt black with black leather
£34,000 58,000 miles



PORSCHE 911 (997) "4S" 3.8 (57 - 2007)
Atlas grey with black leather
£34,000 63,000 miles



PORSCHE 911 (997) "2S" 3.8 (57 - 2008)
Atlas grey with black leather
£34,000 52,000 miles



PORSCHE 911 (997) "4S" 3.8 (57 - 2007)
Atlas grey with black leather
£34,000 56,000 miles



PORSCHE 911 (997) "4S" 3.8 TIP (57 - 2007)
Meteor Grey with black leather
£33,000 62,000 miles



PORSCHE 911 (997) "2S" 3.8 TIP (07 - 2007)
Meteor grey with black leather
£32,000 48,000 miles



PORSCHE 911 (997) "4S" 3.8 (07 - 2007)
Atlas grey with stone grey leather
£32,000 69,000 miles



PORSCHE 911 (997) "2S" 3.8 (06 - 2006)
Arctic silver with black leather
£32,000 40,000 miles



PORSCHE 911 (997) "2S" 3.8 TIP (06 - 2006)
Midnight blue with ocean blue leather
£32,000 31,000 miles



PORSCHE 911 (997) "2S" CAB 3.8 (07 - 2007)
White with black leather
£32,000 53,000 miles

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HOLD PROMISE

In his first article for *GT Porsche*, experienced driving coach, Neil Furber, discusses steering grips and techniques for performance motoring.

Steering is the ideal place to begin my series concerning driving techniques and vehicle technology. Although I'd normally start any driver coaching session with a discussion about seating position, it's the driver's interaction with the steering wheel that often requires the most attention. Believe me when I say it really can make all the difference!

I don't wish to gloss over the importance of finding the 'ideal' setup with respect to

the rest of the seating recipe, but since some older Porsches have limited seat adjustment, I'll always favour an optimised driver-to-wheel relationship whenever possible. After all, some of the greatest pleasure gained from driving a Porsche comes from the way it handles bends. Tyres and chassis respond best to smooth-flowing input; improving your grips and techniques will enable you to optimise movements and receive the best information back from the rubber wrapped around your car's wheels.



DRIVING FORCE

Introducing *GT Porsche's* resident driving expert, Neil Furber. With a technical background as a mechanical engineer in the Formula One industry, Neil brings a unique technical insight to driver coaching. Splitting his time between the French Alps and the UK, he coaches drivers through his brand, Drive 7Tenths (drive7tenths.com), and is also a Porsche Driving Consultant at Porsche Experience Centre Silverstone.

REACH, RAKE AND GRIPS

What's the best position for the steering wheel and how should it be held? My personal preference is for hands at the quarter-to-three position and just below the shoulders, with both arms showing a bend a little larger than ninety degrees at the elbows. For recent models in the Porsche range, lots of adjustment exists, but in my experience, many owners fail to realise they can adjust their car's steering wheel both up/down and in/out to suit their individual dimensions. It's certainly worth spending time re-evaluating your seating position and trialling different settings for the rake (up/down) and reach (in/out) angle. Sometimes, a small change in backrest angle can help, but

there should be enough range to copy with all but the most extreme cases.

As a quick check to ensure things are in the right place, relax your shoulders back into the seat and let an outstretched arm fall gently on the centre-top of the wheel rim. If your wrist is on the rim, you've achieved the ideal position. If you find your palm, fingers or forearm in contact, I'd suggest having another go.

In terms of grip, there's nothing wrong with the ten-to-two position. Many drivers prefer this. I've come to favour the quarter-to-three grip, however, for three important reasons. Firstly, there's a little more range available at the limits of the 'fixed grip' technique (see separate boxout). Secondly, the slight extra

spacing between hands reduces muscular effort during steering. Thirdly, the hands fall naturally to the indicator stalks and paddles now fitted to the majority of Porsches equipped with PDK transmission.

I'm certainly no fan of one-handed grips, asymmetric hand positions or palming the wheel. My favourite example of bad technique is the one-armed bandit – the classic 'reclined seat and one straight arm with hand at the top' motorway cruise pose. You know the one I'm talking about! This is often justified as being 'comfortable'. In truth, the pose usually stems from fatigue in the arms and shoulders when the steering wheel is too far away.



▲ **Good technique** A quarter-to-three grip is a good place to start when evaluating and optimising steering control.



▲ **One-armed bandit** This classic motorway cruising pose is the perfect example of what not to do when behind the wheel of your



▲ **The pretzel** We can discuss the detail of 'fixed grip' steering on the following pages, but this is the technique taken to its limit!



▲ **Wrist check** Distance is good if your wrist can sit on the wheel rim when your shoulders are positioned back in the seat.

THREE CORE TECHNIQUES

Driving isn't a case of right versus wrong. It's more a case of good, bad, better and best. Simply put, finding a better way of doing things will help to create an improved driving experience. A combination of small changes can be greater than the sum of their parts. I'll introduce you to three core steering techniques across the following pages. Once mastered, these top tips can be blended when necessary.

FIXED GRIP

This is the technique most commonly used by race drivers and offers the best feel and control when small steering angles are used. The hands are 'fixed' to the wheel and rotate it without changing grip. When looking to create those silky-smooth steering inputs for high-speed cornering, fixed grip steering is certainly my first choice. The first few

degrees of steering are the most important to help let the tyres and chassis start to work before the bend develops fully. The counterbalanced nature of two opposing hands on either side of the rim allows a level of finesse unrivalled by any other technique. It's ideal for up to ninety degrees of steering wheel movement, but, at the limits, can reach

half-a-turn, or even a little more with a well-placed thumb if absolutely necessary. Relying too heavily on the limits of this technique can make your arms look like a pretzel, though! It also leads to a loss of control. A pre-loading of the wheel (hands preadjusted) or the 'pull-push' technique outlined below offer a better solution in these cases.



1 A quarter-to-three grip gives the best feel when initiating a turn.



2 The 'fixed grip' technique is great flowing bends up to a quarter of a turn.



3 This image shows where things become less comfortable and control-limited.

PULL-PUSH

Not to be confused with the learner-style 'shuffling', the 'pull-push' technique offers great benefits for smooth steering when you'll need between ninety degrees of steering wheel movement and full lock. Alternate hands feed the wheel in half-turns by 'pulling'

down from the top or 'pushing' up from the bottom. It's an ideal method for tight bends, exiting T-junctions and low-speed car park manoeuvring. Although the staple technique for the majority of 'advanced' drivers trained in law enforcement and civilian circles,

it'll never offer the refinement and finesse achievable with 'fixed grip' steering, but is certainly a technique worth mastering for the right situations. It's not the best technique for skid control, though. Far too slow! This is where 'rotational' steering comes into play.



1 Note the position-leading hand placed at the top of the wheel rim.



2 'Pull' down smoothly with the wheel sliding through the other hand.



3 Continue to pull until hands meet at the bottom, then swap hands.



4 'Push' the other side of the steering wheel up with your second hand.



5 Continue to push until both hands meet at the top of the steering wheel.



6 Swap hands again and continue if you feel more lock is needed.

ROTATIONAL

I'm sure many of you will have been told to stop crossing your arms at some point. Rotational steering is just that. The hands change grip around the rim, one after the other following a rotational movement. I've sat next to plenty of drivers who use this technique to negotiate junctions and tight bends. None of the individuals I've observed have looked comfortable. In fact, once we've worked on 'pull-push' steering, they immediately prefer that

technique! Rotational steering is best for when very high steering angles are needed quickly. It's useful for manoeuvring in tight spots, where you may need to move between full lock left to right and when you find yourself dealing with large oversteer skids.

The beauty of the technique comes from it starting as 'fixed grip' steering. If necessary, crossing a hand over to grab the next armful of lock to regain control of a skid can be

done seamlessly. Once truly mastered, using rotational steering will make it easy to find the 'straight ahead' position once a skid is back under control. The secret lies in where to hold the wheel throughout the technique.

We'll take a look at the finer points of steering in a future *GT Porsche* article focusing on cornering, but if you'd like more information on the core techniques outlined in this article, visit drive7tenths.com and view my detailed tutorials.



1 Rotational steering starts with the 'fixed grip' technique, so ensure you're working with a quarter-to-three grip of the wheel.



2 Now turn the steering wheel. Once passing the quarter-turn position, the leading hand lets go.



3 As illustrated in this photograph, the leading hand crosses after letting go of the steering wheel.



4 The leading hand then grips the rim of the steering wheel.



5 Flip the other hand.



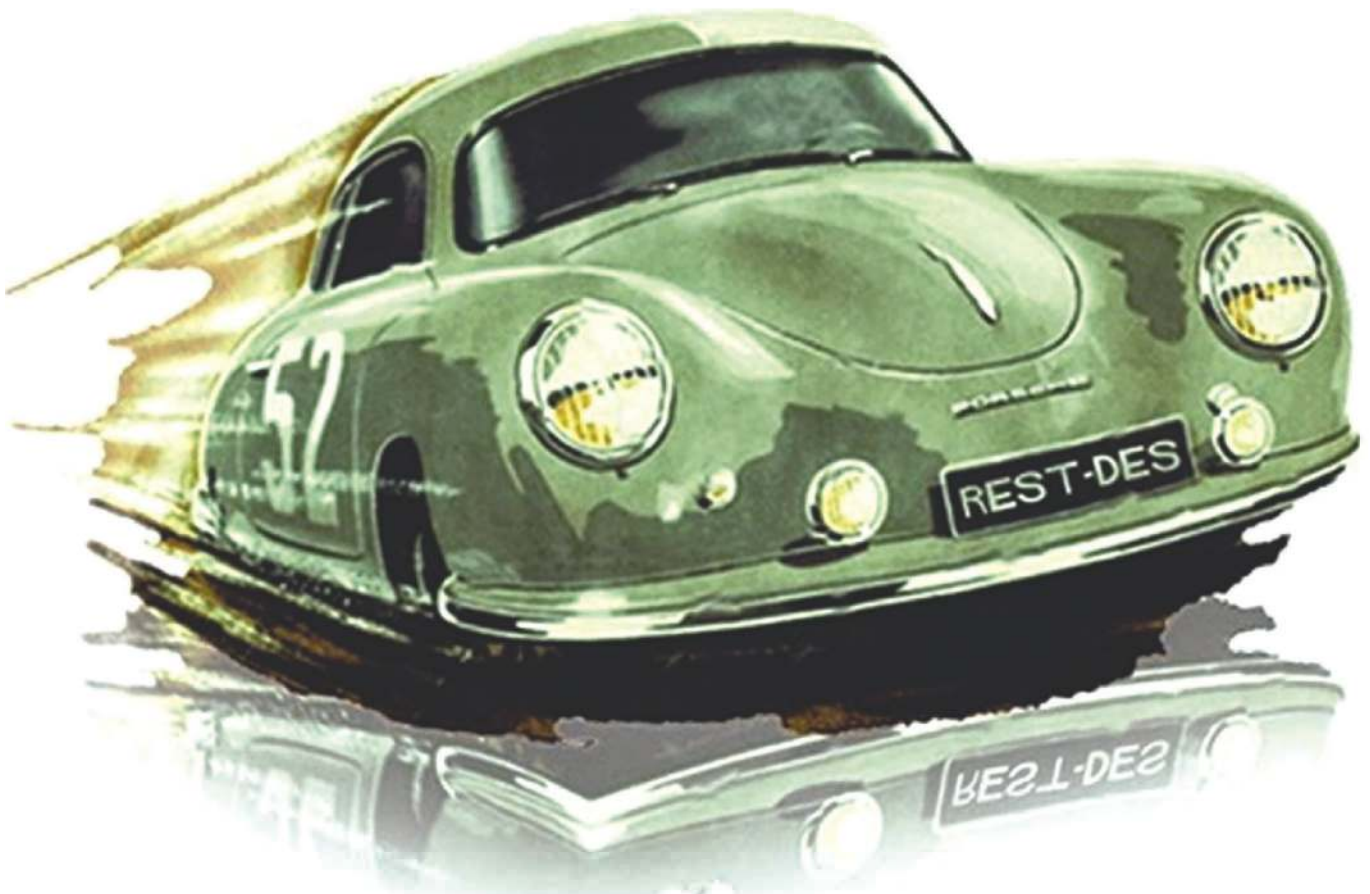
6 Continue with 'fixed grip' steering towards lock stops. You've just completed this month's *GT Porsche* driver coaching tutorial!

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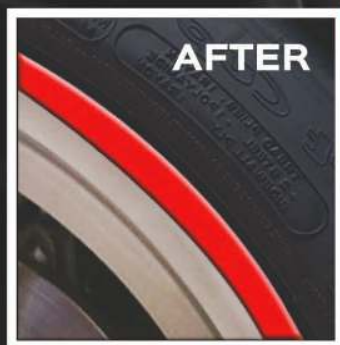
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MEXICAN MAGIC

Motorsport success delivers desirable silverware, but every now and again, the reach of a race extends far beyond a stuffed trophy cabinet...

Words **Richard Gooding** Photography **Various**

The 24 Hours of Le Mans is widely recognised for playing a starring role in Porsche's story of success, but there are less well-known motorsport events which have contributed to our favourite manufacturer's lasting legacy. The Targa Florio endurance race, for example, left an enduring mark on the Porsche product portfolio by inspiring a now-iconic nameplate, as did the Carrera Panamericana, a road race in Mexico held for five consecutive years in the middle of the last century.

A formidable five-day run along the Mexican

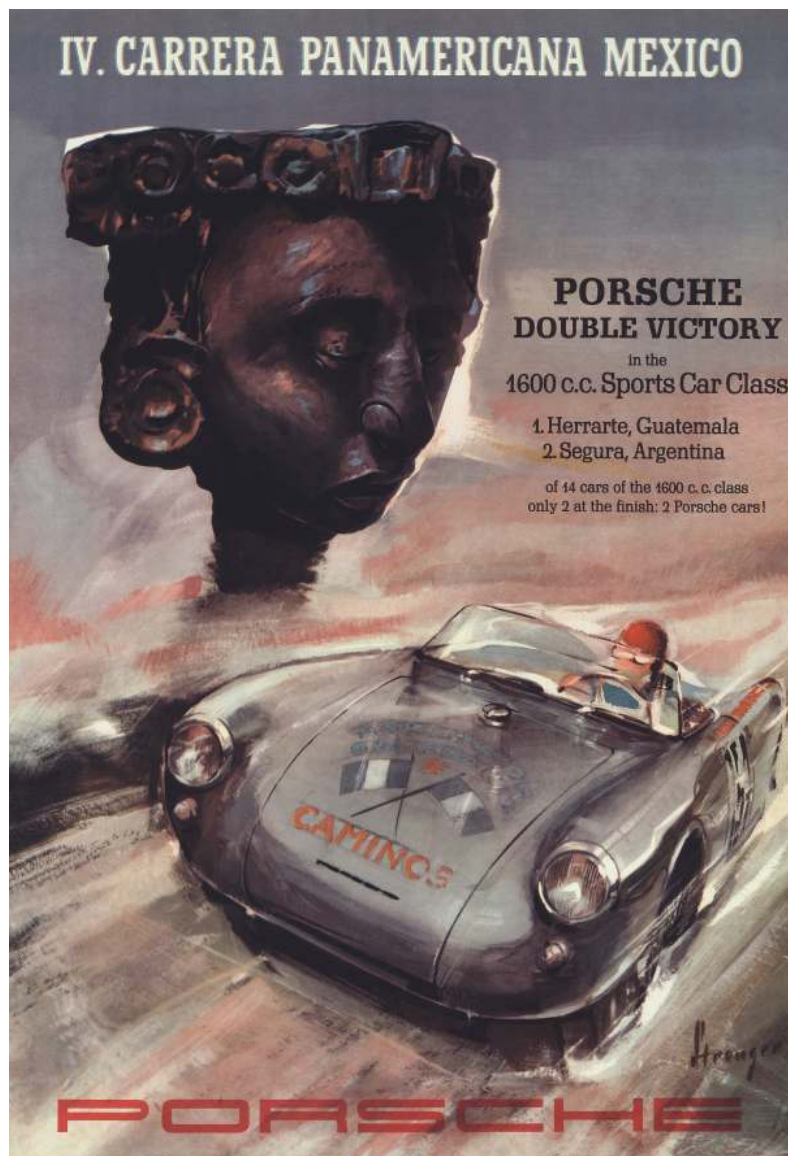
stretch of the Pan-American Highway, the first Carrera Panamericana started on 5th May 1950. A sizeable cash prize was the reward for winning, but the lure of adrenaline-packed, high-octane action and the promise of positive PR as a consequence of simply turning up to compete proved irresistible for many major sports car manufacturers, leading to grids packed with internationally renowned racing drivers rubbing shoulders with local amateurs.

Intended to offer a viable alternative to established racing events in Europe, the Carrera Panamericana was

Above One of two 356s that took part in the 1952 race, each vehicle supported by Herbert Linge in the role of chief spanner-wielder

STOMPING GROUND

The Carrera Panamericana name was revived in 1988 when a yearly, seven-day trek covering 2,000 miles along sections of the original race route was established. In High-speed, closed stretches of public highway are the main attraction.



widely regarded as the most dangerous competition of its kind due to challenging terrain and sudden elevation changes. Desolate jungle added to the risk of the 2,178-mile route, which is partly why the event was a big draw for experienced drivers – men and women – keen to challenge themselves. Indeed, entrants included stars of Formula One, NASCAR, hill climbing, drag racing and rallying, although race regulations restricted the type of vehicle allowed to participate in 1950's outing to the humble five-seat sedan.

DETERMINED TO WIN

Alfa Romeo constructed cars specifically for the inaugural Carrera Panamericana, a move paving the way for the acceptance of sports cars on the entry sheet in 1951. Consequently, a Ferrari 212 Inter Vignale raced to the top spot, an accomplishment which further raised the contest's profile and saw it become a highly anticipated fixture of the international racing calendar in 1952. Mercedes-Benz wanted a piece of the action, resulting in three 300 SLs hitting the grid. The same year signalled the starting salvo for Porsche's participation. »



NO FEWER THAN TEN OF THE STUTTGART SPORTS CAR MAKER'S CREATIONS LINED UP ON THE GRID

The 356 had already proved itself capable of winning races thanks to victory in the 1,100cc class at Le Mans in 1951. From that moment, Porsche and motorsport were inextricably linked. Overseas, American car culture spawned an army of club-level racers and encouraged an enthusiastic racing scene. Porsche recognised the potential for welcome showroom sales as a direct result of this activity, and with drivers of little Porsches scything through state passes while teasing larger, more powerful machinery, it was obvious the manufacturer needed to have a presence at the Carrera Panamericana.

Rule changes for 1952 included the creation of Stock and Sports categories intended to level the playing field, thereby ensuring lightweight European cars weren't competing directly with heavy American four-doors. Additionally, the race doubled up as the last round of the World Sportscar Championship. A pair of 356 Supers set off from the starting line, one of them with Porsche technical luminary, Herbert Linge, as co-pilot and mechanic. Cars wearing the three-pointed star, however, dominated, taking first and second place. The third SL was disqualified for "illegal repairs" on the penultimate day of the race, a ruling stopping the boys from Benz scoring a 1-2-3 finish.

The 356 Super Cabriolet driven by Prince Paul Alfons von Metternich-Winneburg, a racer who would go on to become head of the FIA in 1975, finished eighth in the Sport class. It was enough to encourage Porsche to return to Mexico in 1953, when no fewer than ten of the Stuttgart sports car maker's creations lined up on the grid. A pair of works-backed 550 Spyders, one piloted by the previous year's Carrera Panamericana winner, Karl Kling, were paraded before spectators. Unfortunately, Kling's car failed to finish due to a busted driveshaft, while the other 550 Spyder, driven by works star, Hans Herrmann, suffered an accident. It was left to privateers to deliver the goods.

DEADLY AMBITION

The ex-Le Mans 550 of Guatemalan Porsche distributor, Jaroslav Juhan, performed well, leading the Sports class until regrettable mechanical failure handed the spoils to his fellow countryman and 550 Coupe driver, Jose Harrarte, who finished first in class and thirty-second overall with a time of 23:57:04. Many of these headlines, however, were overshadowed by the deaths of spectators who were hit by the Ford 6 of Mickey Thompson (he of Bonneville Salt Flats and performance tyre fame) after it crashed at the same spot as the Ford 6 driven by Bob Christie a short while beforehand. Spectators who had moved to »





Above Hans Herrmann posing in the one of the works 550 Spyders before the fourth Carrera Panamericana



Facing page UK-born Mexican film actress, Jacqueline Evans, in the 356 Coupe she campaigned during 1953's race



WHAT'S IN A NAME?

In Spanish, Carrera translates as 'career'. Porsche adopted the nametag to denote the presence of a race-inspired powerplant following the installation of Ernst Fuhrmann-designed Type 547 quad-cam engines in the 1954 Carrera Panamericana 550s. Subsequently, the 356 A 1500 GS Carrera, the 904, all generations of the 911, the 924 Carrera GT and the 2003 Carrera GT have all worn the badge.

Production models may have taken the Carrera element of the

famous race's name, but Porsche's 1989 964 C4-based concept car was the first to adopt the Panamericana tag. The strange-looking Targa was built for Ferry Porsche's eightieth birthday, featured seamless lines and bold 'exposed' wheel arches promoting airflow and easy wheel changes. The composite-constructed car included a three-piece removable roof held into place by purple zips. The finished concept would go on to influence the design of the 993-generation 911.



HAND-PAINTED ONTO EACH CAR'S CURVACEOUS FLANKS BY WAY OF STENCIL, BRIGHT BRANDS CAUGHT THE EYE

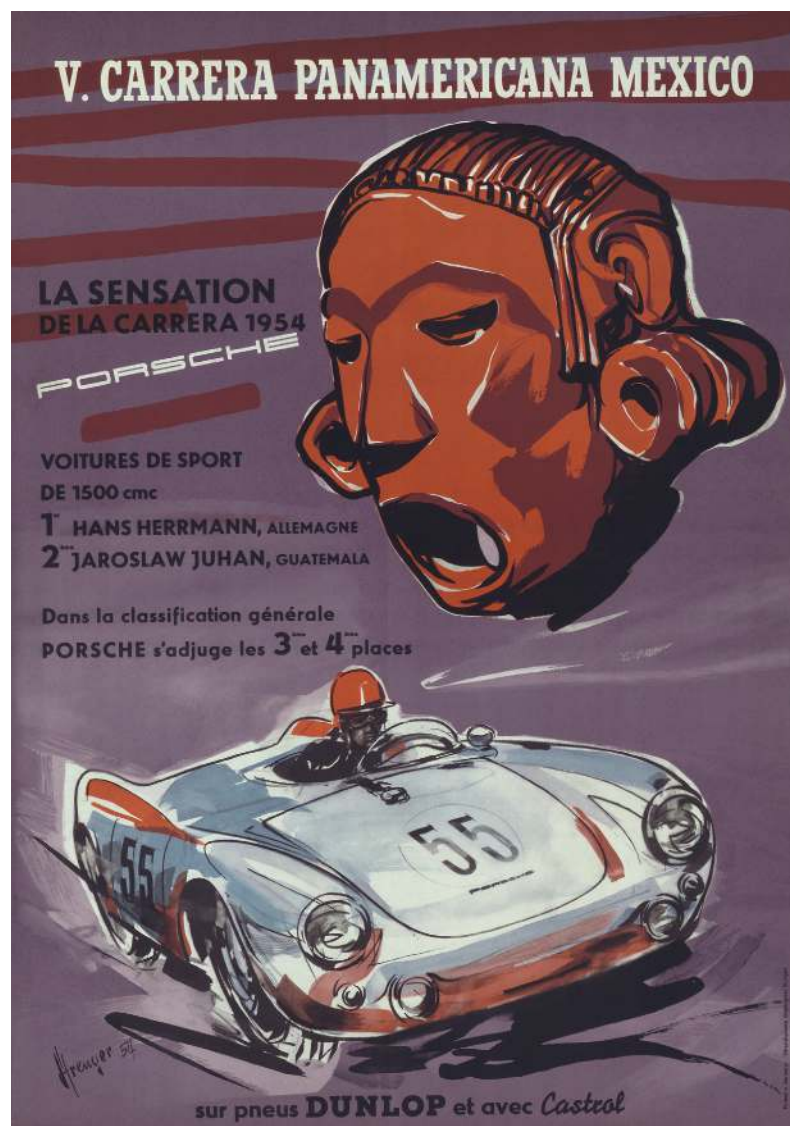
witness the recovery of Christie and his co-driver, Kenneth Wood, had unwittingly put themselves in the direct line of Thompson's car, which was suffering failed brakes. The car flew straight into the embankment, killing six and adding to the race's death toll of nine, including Italian racing drivers, Felice Bonetto, Antonio Stagnoli and Giuseppe Scottuzi, all who crashed into buildings at high speed.

For the 1954 Carrera Panamericana, now a highly professional pursuit, Porsche put together what many saw as a winning package. Hans Herrmann was once again drafted in to bring silverware to Stuttgart by way of campaigning an overhauled version of the 550 Spyder, complete with revised rear suspension and a new engine. The debut of the Fuhrmann-developed Type 547 four-cam flat-four racing unit – thereafter known as the Carrera powerplant – propelled Herrmann to victory in the 1,500cc class, but could only manage third place overall, pipped to first and second position by Ferrari 375s. Despite respectable results – Juhan finished fourth in a 550 Spyder – the race wasn't without incident for Porsche, with cars arriving in Mexico late and Herrmann battling a fever. An eye-watering South American mixture of honey, a half-litre of cognac and a splash of milk was downed by the plucky German, who had little faith in the toxic tonic. Fortunately for him, it worked, enabling 550-04 to take to the starting line with its celebrated works pilot in fine fettle.

Linge acted as mechanic and would prove to be Stuttgart's secret weapon after Herrmann suffered a setback from the off; as well as an engine misfire, the tread on his newly-developed tyres began to disintegrate. In his infinite wisdom, Linge had the foresight to recognise the location of a tyre depot along the route. Herrmann reached it after seventy kilometres of motoring on worn-out rubber! From that point onward, it was full steam ahead; defeating cars twice its size, the 550 Spyder romped to third overall and first in class. With an average speed of 97.63mph, plus a time of 19:32:33, the lightweight roadster and its new engine were more than a match for brawnier competition. Juhan crossed the line in fourth, with both his and Herrmann's 550s crossing the finish line alongside one another.

CELEBRATING INNOVATION

Race winner, Umberto Maglioli, would pilot a 550 A RS Spyder to Porsche's debut win at the Targa Florio two years later, but until the Italian secured victory in Sicily, the 1954 Carrera Panamericana was Porsche's greatest



Facing page
The Carrera Panamericana route is a popular drive for fans of historic motorsport, and though the race ceased to exist in the mid-1950s, its legend lives on in the form of a wide variety of Carrera-badged sports cars and surviving vintage racing machines, many driven in the present by current and former Porsche works racing drivers

international racing success. The giant-killing 550s were hard to miss; in a break from tradition, Porsche allowed the display of sponsor logos. Hand-painted onto each car's curvaceous flanks by way of stencil, the bright brands of Fletcher Aviation, Telefunken and Castrol caught the eye. 'Racing Baron', Fritz Huschke von Hanstein, Porsche's public relations manager and head honcho of its motorsport division, convinced the brands to back both 550s. The promotional opportunities not only broke new ground for the manufacturer, but also for motor racing in general, which hadn't previously witnessed such carefully considered on-vehicle advertising.

Sadly, as cars became faster and racing increasingly aggressive, motorsport's deadly reputation meant the Carrera Panamericana's race was won. Over just five events, it achieved one of the highest mortality rates per race! Nevertheless, attracting driving royalty, including Formula One champions, Juan Manuel Fangio, Aberto Ascari and Phil Hill, as well as an astonishing two-million spectators, the Carrera Panamericana proved pivotal in Porsche history, delivering an iconic nameplate used on many of the manufacturer's most popular production cars. From souped-up 356s to the Carrera GT, it's safe to say this evocative tag remains firmly on the wild side.

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THREE OF A KIND

Whether you're a fan of stock specification or heavy modifying, there's no denying the 964's ability to cater for a wide range of driving styles

Words **Dan Furr** Photography **Dan Sherwood**





Above Not what you were expecting to see in your car's rear-view mirror

It's a dull day in May and we're at a location once considered so secret it was omitted from Ordnance Survey maps. We're deep in the Buckinghamshire countryside, but in place of tall trees stand towering curves of scorched concrete stretching skyward. A scattering of deteriorating buildings and lofty observation posts are filled with redundant heavy machinery and control panels that bear more than a passing resemblance to the original TARDIS console. It is, then, not entirely surprising to discover the crude dials and levers we're staring at were designed to manage advanced missile propulsion. More specifically, the long-redundant hardware was operated by scientists instructed by the British military to advance the V1 and V2 liquid-propellant rocket engine programmes developed in Germany during World War II.

A crescendo of barking exhausts fills the air as ballistic German weaponry of a different kind approaches the site's surviving test stands. Grey cloud is all but forgotten as a vibrant, moving palette of Amazon Green, Maritime Blue and Guards Red rapidly draws near. Anybody watching from afar would be forgiven for thinking military operations were firing up again, but a closer look reveals the form of three 964s hammering along the wide, flat roads snaking their way around the vintage research and development facility.

It's strange to think the 964 was once the unloved 911, such is the level of appreciation for the model today. Now regarded as the 'sweet spot' of the air-cooled era, the 964's blend of classic styling, mechanical reliability, performance poke and creature comforts makes »

it the go-to 911 for most companies specialising in air-cooled Porsche restomods. DIY modifiers have also long held the 964 in high regard, yet we don't need to venture too far back in time to witness many surviving examples suffering neglect as a consequence of low vehicle value allowing buyers without the means to properly maintain their Porsches leaving knackered suspension, major oil leaks and bore score to flourish before catastrophic engine failure put paid to any more fun on four wheels.

One of the plus points of the 964 once being the low-cost entry to 911 ownership is that prior to the explosion of air-cooled Porsche purchase prices, many owners weren't deterred by the idea of moving away from factory specification. There simply wasn't a huge amount of money tied up in the 964s these guys and girls owned, making the threat of compromised resale value something they didn't need to be concerned with. Fast-forward to the present day, and prices of all 964s have shot up almost as sharply as the long-range missiles developed where we find ourselves standing. Even so, encouraged by technological advances in the development of aftermarket performance parts, not to mention the manufacturer's growing celebration of many independently modified 964s, there remains an army of owners only too happy to see their 964s ripped apart and put back together in a custom state of tune.

THE MAGIC NUMBER

The trio we've gathered together represent very different approaches to 964 personalisation. Each car offers a different driving experience to the next, with the most extreme of the bunch being James Stewart's brilliant blue RS evocation. By its proud owner's own admission, the stripped and sprayed track-friendly road racer is far

IT'S DIFFICULT NOT TO FEEL AS THOUGH YOU'RE AN INTEGRAL PART OF THIS STRIPPED-BACK 964

from the most powerful 964 you'll ever come across, but the ridding of much weight and a heavy focus on chassis optimisation has resulted in an extremely aggressive ride from what was once a tame Carrera 2 carrying more filler than you'll find in your Nan's bathroom.

"It was a category D write-off following an accident in 2001," explains James. "A pre-purchase inspection highlighted rear quarter damage and a body bent like a banana. The roof had also been replaced at some point in the car's life." Determined to reconstruct the cool Carrera to his own tastes, he bought the poorly Porsche and commissioned corrective surgery, giving Raikku at air-cooled engine specialist, Waff Zuff, the job of stripping the car to a bare shell before the monocoque was delivered to Prime Paint & Body in Harlow for correction. Metal was straightened, accident damage was eliminated and fresh paint was applied in readiness for a return to the road.

The rebuild was carried out with James' love of the 964 RS firmly in mind. From the clutch and flywheel to its rear glass, the great pretender was littered with parts sourced from Jarny-based retailer, Rose Passion, with an engine service kit, brake lines, window seals and an RS quick shifter forming an impressive stockpile. H&R anti-roll bars, Bilstein PSS10 coilovers, Rennline adjustable rear spring plates, GT3 engine mounts,



STATES OF MIND

Introduced in 1992, the 964 RS was based on the 911 Carrera Cup race car and featured an updated version of the standard naturally aspirated 964 engine. Stripped and track ready, the RS wasn't sold in the USA, although an 'RS America' running a standard Carrera 2 powertrain was introduced to the market in 1993.



assertive geometry and Powerflex polyurethane bushes completely transformed the car's handling. New genuine Cup wheels wrapped in Michelin Pilot Sport rubber and hiding 993 calipers loaded with 928 S4 discs were also added to the mix.

The car's 3.6-litre engine remains unopened, but benefits from Rothsport Racing pulleys, Magnecor ignition leads, a catalytic converter delete, a Cup pipe and a chipped ECU. This is by no means an RS evocation chucking out big bhp, but pairing the car back to minimal equipment and replacing whatever was left with lightweight alternative parts (Recaro Pole Position buckets are fitted where heavy factory sports seats once lived, while lightweight RS-spec carpets take the place of bulky production pile) has ensured seat time just as awe-inspiring as the high-octane action James enjoyed back in his friend's 996 at Goodwood.

As you'd expect from a track-focused Porsche, stepping in and out of the car isn't the most straightforward of tasks, but once buckled into the Schroth harnesses and surrounded by the Heigl roll cage, it's difficult not to feel as though you're an integral part of this stripped-back Stuttgart sports machine. Sat low on the wound-down Billies (and feeling lower still thanks to the big-bolstered Recaros), the chassis keeps you »

Above Maritime Blue is definitely on our shortlist of the best 964 colours



Facing page A vote of approval from Urban Outlaw, Magnus Walker, is scribbled on the car's bodywork

in touch with every bump and vibration. It's a firm ride delivering a thoroughly engaging drive and one we have no doubt comes into its own when being put to the test at a circuit. Alas, we have little opportunity to feel the full force of this poised Porsche while zipping along the service roads winding around the amazing venue we're using for our photo shoot, but the sharp corners and tight turns punctuating the long stretches of flat tarmac are enough to make us aware of the car's instant recognition of driver input, where small adjustments to steering and light pressure on the throttle make a bigger difference to how the car behaves than one might ordinarily give a 964 credit for. Just like the colour that covers it, there's nothing subtle about the way this RS-inspired build conducts itself.

Living with an air-cooled 911 as combative as the 964 James owns isn't for everyone. Long distance driving, for example, might be too loud, too uncomfortable, too raw for many who prefer their Porsche to retain civilised road manners when it isn't being driven hard. The 1990 Guards Red Carrera 2 Steve Bennett has driven to mission control is a fantastic example of a 964 offering the best of both worlds, where standard seats and luxury trim sit happily alongside hardcore chassis upgrades. "I conducted a lot of research to determine the most technologically advanced suspension system available for the 964," he tells us. "I'd already fitted

Bilstein B6 dampers and lowering springs, but I wanted adjustability to suit different driving conditions." He also didn't want to faff around underneath his air-cooled classic every time he wanted to change bump and rebound settings, which is why he enlisted the services of technicians at Tractive Suspension, a performance coilover manufacturer located in Cuijk, a municipality in the upper southeast Netherlands.

SHOCK AND AWE

"The damper bodies are all alloy, making them significantly lighter than similarly marketed parts produced by other suspension makers," confirms Steve. Tipping the scales at an impressive twenty-five percent of the weight registered by more commonly seen kits, the Tractive package is supplied with its own software and a wiring loom enabling connectivity to a DSC Sport digital damper control interface, affording Steve the ability to switch between three different suspension settings at the touch of a button. He's set up touring, B-road and track maps accordingly.

The shocks react almost instantly to what's being asked of them, changing damping rates at less than a sixtieth of a second. The electronically controlled changes – suitably, Tractive refers to the system as ACE (Activity Controlled Electronic) – allow Steve's 964 to travel completely flat through corners, improving grip

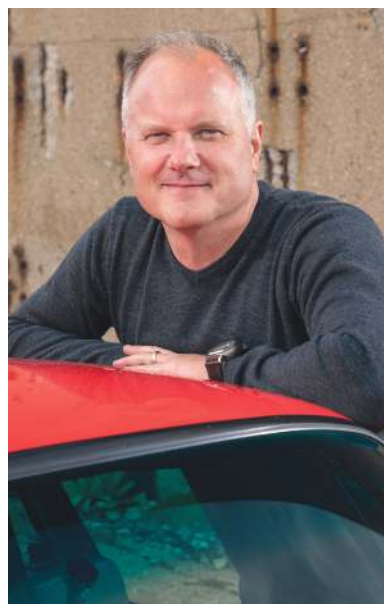
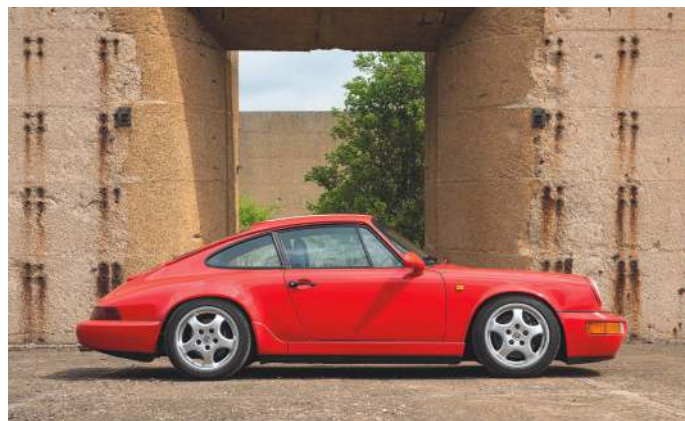
Right Flat-six remains unopened thanks to a lifetime of the proper care, leaving Steve to modify without concerns about engine reliability

Below Able to act as tame as it does aggressive, this Carrera 2's Tractive suspension gives it a split personality

POWER TOOLS

The 964 was launched with a new 3.6-litre flat-six. The model also featured an electric rear spoiler (raising itself at 50mph before returning to sit flush in the engine lid when speed slowed), ABS and power steering as standard equipment.





WHERE STANDARD SEATS AND LUXURY TRIM SIT ALONGSIDE HARDCORE CHASSIS UPGRADES

on the fly through braking, accelerating and changes of direction. They also allow the car to be compliant over undulating asphalt.

Working with the guys at Tractive and with help from Porsche suspension wizard, Chris Franklin (he of Centre Gravity fame), Steve worked hard to provide real-world feedback to the aftermarket manufacturer regarding its damper design, spring rates and the kit's control unit. Subsequently, the rear shocks were altered to eradicate front-to-rear droop and to take up slack in the helper springs. Separately, in order to achieve totally uncompromised handling and to get the best out of the Tractive package, Steve called upon Santa Clara Porsche parts specialist, Elephant Racing, to supply adjustable top mounts, rear monoball spring plates, adjustable drop links and a bump steer correction kit.

Chris moved the front wishbones out by 30mm on each side (thereby improving turn-in and producing a squarer overall track), although comfortably fitting everything Steve was buying was a tricky task thanks to his decision to install Eibach hollow adjustable anti-roll bars and bigger braking equipment, including 928 'Big Black' anchors, a 993 master cylinder and a 993 brake booster kit with braided lines.



Like James' Carrera 2, Steve's car makes use of many RS-spec components, including an RS clutch and flywheel, a lightweight rear window and RS engine mounts to ensure better control of mass at the rear through fast corners. Cups and Michelins (Braids and Toyos for the summer) fill the wheel arches, while the tip of an exhaust fabricated by Bicester-based historic motorsport specialist, Ryan Edwards, peeks out from the rear. "OEM 964 primaries are a considerable barrier to ensuring free-flowing gases," he frowns. "Furthermore, the efficiency of the standard parts is compromised in order to allow boxes for heat to be transferred into the cabin. Additionally, the headers aren't equal length, a feature ordinarily only available to 964 owners by fitting race-spec parts and foregoing heating inside the car." In something of a theme for his build, a desire to achieve the best of both worlds saw him work with Ryan to design an advanced exhaust system featuring heat exchangers after the primaries, thereby allowing the creation of equal length manifolds feeding into cylindrical heat exchangers designed to accommodate free-flowing 100 or 200-cell catalytic converters.

REINVENTING THE STEEL

The finished system is made from 304-grade stainless with 1.5mm-thick tubing for the primaries. Each header weighs just 4.025kg, with the entire exhaust saving twenty kilos over the factory system. Built to amazingly high standards and significant development costs, the effectiveness of Steve's pet project has been qualified on the rolling road at tuning firm, Superchips, located a stone's throw from where we're experiencing the

A COMPLETELY DIFFERENT ROUTE TO UPDATING A 964 THAN THE PATH JAMES AND STEVE WALKED

effect of what he's getting ready to market as a product available for all 964 owners to take advantage of. "Superchips tested the car before and after the new exhaust was fitted," he says. "With the full system and an ECU software update in place, the car now delivers 295bhp, which is almost fifty more ponies than Porsche quotes as the power output for a standard Carrera 2." No wonder he thinks there's a huge opportunity to market the system to his fellow 964 owners.

Listening to the modifying enthusiast's plans for world domination, Al Kingsley responds by informing us he's planning to take his all-pawed 964 to Superchips for custom mapping in the not too distant future. To date, he's been more concerned with ensuring the 1992 Amazon Green Carrera 4 is operating without fault and is in pristine cosmetic condition after much time and money spent remedying complaints his car was carrying when his name appeared on its logbook.

"Compression was down on two cylinders," he sighs. A full engine rebuild was carried out at RPM Technik, with the addition of a new oil pump, new earth straps, replacement sound deadening materials and new heat shields. Deciding jobs generated through choice rather than necessity might soften the blow of having to





Above Gloriously retrimmed interior is the work of Awesome and replaces the tired Linen leather inhabiting the car when AI bought it

Left This Amazon Green Carrera 4 isn't track focused, but that doesn't make it any less fun to drive on the road

commission remedial work so early on in his ownership, he asked the RPM Technik team to powdercoat the engine's cooling fan and covers bright red.

The revitalised engine was bench-tested for a solid twenty-four hours before being returned to its natural habitat. While this exercise was taking place, new brake discs and pads were fitted to refurbished standard calipers (now powdercoated red), while the exhaust was upgraded with a sports cat and a G-pipe delivering greater growl. Trouble lay ahead, though, as evidenced by the need for the gearbox to be rebuilt just six months later. Back to RPM Technik the car went, returning to AI only after all bearings, synchros and mounts were replaced. The differential was also given a once-over.

That was eighteen months ago, and in the time that's passed since, AI has enjoyed ensuring his car's appearance is up to the same high standard as its rebuilt mechanical components; Carrera 4 side stripes decorate bodywork that can look blue or green depending on the amount of light hitting it, while genuine new Fuchs in staggered seventeen-inch fitment have been purchased and bolted into place.

One of the biggest changes he's overseen, and one which demonstrates a completely different route to updating a 964 than the path James and Steve have walked along, is the now luxurious specification of his car's interior. Looking at the beautiful black





STAGE SCHOOLED

Porsche claimed the 964 to make use of fewer than fifteen percent of the parts used during Carrera 3.2 production when the newer model was launched in 1989. The first 964s were all Carrera 4s, with the two-wheel drive Carrera 2 arriving in 1990.

upholstery and deep-pile carpets covering most of the cabin, you'd be hard pushed to realise Al bought the car with tired Linen leather and equally grubby floors. Instead of ripping out the 'Tombstones' and replacing them with sportier bum huggers, he decided an OEM+ route to personalisation was in order, commissioning automotive upholstery specialist, Awesome, to retrim the standard seats in black hide with perforated centres and colour-coded stitching. The Awesome boys, well-versed in the art of creating custom classic Porsche interiors, added deep-pile black carpets, a new headlining, gaiters, door cards and overmats at the same time, before covering the dashboard, door card tops, the parcel shelf and any other shiny black plastic in stitched Diamante vinyl. Granted, that last ingredient doesn't sound like it should work, but it didn't take long for James and Steve to join us in expressing a lot of love for the finished interior when Al revealed the awesome fruits of Awesome's labour.

FOUR PLAY

His car is a totally different animal to the 964s it finds itself parked next to during our photo shoot. Apart from anything else, four-wheel drive changes the characteristics of a 911 to a considerable degree, no matter the model. Admittedly, the majority of Al's car's drive is kept at the rear, but the huge amount of extra weight that comes with this type of transmission isn't in keeping with the 'less is more' approach James

Above All three 964s are absolutely brilliant examples of personalised air-cooled classics, but which of the three is your favourite?!

THE 964 PLATFORM CONTINUES TO BE A TEMPTING PROPOSITION FOR MODIFIERS TODAY

favours. Others following similar trains of thought have converted their Carrera 4s to two-wheel drive, but not Al. He's happy to keep his pretty Porsche as a capable cruiser. It's not going to get a sniff of track work, let alone be pushed to the limit of its capabilities. His is a luxury sports car with all the trimmings, and he's in no rush to compromise his enjoyment of the unusually coloured coupe by forcing it to become fiercer, even if the impending trip to Superchips results in livelier throttle response and a few extra horses.

After hopping in and out of each car and enjoying the effects of three contrasting approaches to Porsche personalisation, it's entirely clear to us why James, Steve and Al are committed to tailoring their 964s to suit their individual wants and needs as drivers. This terrific trio of air-cooled classics demonstrates just how versatile the three-decade-old 964 platform is and why it continues to be such a tempting proposition for enthusiastic modifiers and aftermarket parts manufacturers today. Here's to the next thirty years of 964 history. Onwards and upwards!

TRACTIVE

The Art of Suspension



Tractive Suspension is specialized in electronically adjustable suspension. Although the relatively young company only started in 2010 boast a wealth of experience due to their history. It was founded by the core development and engineering team from WP suspension. In their past they developed products used for MotoGP, World MX, Supermotard, Trail, ATV, Snowmobiles and Formula 1. They've contributed to the success of more than fifty GP teams and riders, among these names are Valentino Rossi, Jorge Lorenzo, Max Biaggi, Loris Capirossi, Piere Francesco Chile, Joël Smets and Michael Schumacher. Already in the 80's the team member were active in electronic suspension in racing and in the beginning of the 21st century they developed the first electronic systems for larger automotive customers like BMW. As such Tractive Suspension can literally rely on team members with a life long experience in electronic suspension.

When WP Suspension moved to Austria, the Tractive team aimed to keep this unique expertise and innovation alive. The fresh start in 2010 with plenty of relevant experience offered the opportunity to make shock absorbers using absolute top materials and technologies. Today Tractive stands for attractive products providing superior traction, ultimate comfort, precise adjustability and modern design for aftermarket products as well as special OEM series. The shock absorbers are equipped with all ways of adjustment, from manual single adjustment till the most advanced semi-active electronic way of adjustment.



Tractive offers electronic suspension for a large number of Porsche models in the Road/Track product line. For all these models there is the ability to have a compliant forgiving and comfortable car for daily commute while, with the push of a button, the vehicle can transform to active and responsive for fast road and occasional track days.

For Porsche cars equipped with the PASM system Tractive offer a full replacement kit for the original suspension, including a new controller from DSC Sport. The new suspension plugs directly into the original connectors, this means there are no issues with electronic faults or messages on the dash. It's pre-programmed for each specific model and it is fully controllable through the original controls in your Porsche. The DSC Sport controller comes with software to allow finetuning by its user.

If your Porsche is not fitted with PASM, Tractive offers a standalone electronic system. It's called ACE (Activity Controlled Electronic) suspension. Apart from shock absorbers, the kit contains a wiring harness, controller and touch screen display. The controller contains internal sensors, pending the measurements of these sensors, the adjustment are made to the correct corner. With this system the car's behavior and tire contact patch are optimized giving more control and feedback of the car. The touch screen allows easy access to the controls and five presets for different road surfaces can be saved. This change is possible due to our patented DDA Valve, which can benefit of a large damping adjustment range within 6 to 10 milliseconds.

Tractive Suspension not supplies the Road/Track line, but the systems also fid their way into motorsport. As an example the World Time Attack Challenge 2018 was won by the RP968 running our R-ACE system which features external adjustments for low- and high-speed compression damping. The suspension of the RP968 was also fitted with extra sensors which allowed us to run multiple algorithms to optimize its performance.



Feel free to contact us if you are looking for an upgrade of your suspension or more information about our products.

Tractive Suspension BV, Kovel 1B, 5431ST Cuijk, The Netherlands, autosport@tractivesuspension.com

987 CAYMAN (2006-2009)

Prices of early Caymans remain low, making now a good time to grab keys to the Porsche you always promised yourself

Words **Dan Furr** Photography **Dan Sherwood**

The 986 Boxster was a phenomenal sales success, but as the turn of the century got underway, the model's appearance was beginning to look dated. A facelift in 2002 helped maintain the mid-engined roadster's appeal, but these changes pale into insignificance compared to the major updates arriving with 987 nomenclature a short while later.

Revealed alongside the then new 997 at the 2004 Paris Motor Show, the new Boxster looked fantastic and was a big hit with buyers, yet the void between Boxster and 911 ownership remained painfully wide. After two years of intense development work, the Cayman S bridged the gap. Unveiled at the highly anticipated 2006 Frankfurt Motor Show, the cool coupe was based on the 3.2-litre Boxster S, but featured a 3.4-litre flat-six wearing 997 cylinder heads. The new model was every bit a 'baby 911', and did its best to prove as much by demonstrating a 0-60mph time of a smidge over five seconds.

If anybody needed further convincing of the new model's performance credentials, Porsche brand ambassador and two-time World Rally Championship winner, Walter Röhrl, put minds at rest by using a Cayman S to lap the Nürburgring four seconds quicker than he was able to complete the same task from within the confines of a standard 911. Admittedly, his 987 was fitted with nineteen-inch wheels, made use of Porsche Active Suspension Management (PASM)

and was equipped with Porsche Ceramic Composite Brakes (PCCB), but, according to the manufacturer's promotional literature, even in stock specification, the Cayman S can finish the same time trial little more than five seconds slower than the Carrera.

The lesser powered, non-S Cayman was launched in summer 2006. Featuring a 2.7-litre engine boasting better fuel efficiency, the new arrival was just as practical as its higher-powered sibling, with the common 987 Cayman design including a hatchback to enable easy access to luggage space. Both versions of the cool coupe share many of the same-age Boxster's key components, including bumpers, doors, lights and much in the way of cabin furniture. Exterior styling took heavy influence from classic Porsche racing machines, as well as the legendary Carrera GT.

A six-speed manual gearbox was the default option for the Cayman S, while the non-S made use of a five-speed manual transmission. A Tiptronic 'box with five cogs was available to order for those who fancied more leisurely cruising or driving around town. It's here where the compact coupe shines; just like the Boxster it takes its core design from, the early 987 Cayman (S or non-S) is a brilliant everyday sports car, offering excellent build quality, great comfort and welcome practicality with running costs at the lower end of the sports car spectrum.

The 987 range was revised in time for the 2009 model year, but it's the earlier S and non-S 987 Caymans we'll consider in this buying guide.

Right 3.4-litre 987 Cayman S is a more focused car than the 2.7-litre model

Below Atlas Grey Metallic was a popular choice and suits the Cayman's Carrera GT-inspired lines perfectly



SIX OF THE BEST

The 987 Cayman S's 3.4-litre flat-six was a development of the 3.2-litre 987 Boxster S engine, using cylinder heads from the 997 Carrera S's 3.8-litre VarioCam Plus unit.



A TIPTRONIC 'BOX WITH FIVE COGS WAS AVAILABLE TO ORDER FOR THOSE WHO FANCIED MORE LEISURELY CRUISING AROUND TOWN

ENGINE

A lot has been said about the reliability of older water-cooled flat-six production engines. To separate fact from fiction, we spoke to Grant Pritchard, Managing Director at Porsche engine specialist, Hartech.

"Unlike early Boxsters, Caymans rarely suffer from Intermediate Shaft (IMS) bearing failure," he tells us. "The complaint was caused by Porsche fitting a bearing that wasn't big enough. Thankfully, production cars received a fix in the form of a bigger bearing for the 2006 model year."

He goes on to suggest timing chains can snap prematurely ("it's extremely rare, but it does happen occasionally") and that the Cayman S's 3.4-litre engine is prone to bore score and cracked

cylinders. "The fact of the matter is that even the newest first-gen 987 Cayman is a decade old, meaning wear and tear to mechanical components will need addressing."

The crankshaft bearing can let go regardless of the early Cayman you own. Prevention (and cost), as the saying goes, is better than the cure, plus corrective engine work offers the opportunity to invest in enhanced performance.

"Hartech regularly increases the displacement of 3.4-litre flat-six engines to 3.9-litres," smiles Grant. "The results are amazing, delivering a massive increase in torque and horsepower, while ensuring longevity through the replacement of all operating components susceptible to failure through old age," he adds.





HISTORY

Make sure you paw through all documentation supporting claims of servicing and maintenance in accordance with Porsche's recommended intervals. Demand to see receipts or invoices proving the seller's suggestion of repair work and replacement parts. Be wary of any unexplained periods of the 987 you're looking at being off the road. If in doubt, have a full inspection carried out by a marque specialist. If you're in an owner's club, check to see if this is a service you can take advantage of as a benefit of membership.

Enter the Cayman in question's details into the DVLA's online vehicle enquiry service (visit bit.ly/dvlaenquiry). It's free to use and will give you key information about the Porsche you're looking at. Additionally, take a few minutes to view information held on the DVLA's MoT history database (bit.ly/dvlamot). Another free service, it'll provide you with details of all passes, fails, advisories and mileage at the point of each test. Check to make sure the details match what's listed on the car's accompanying paperwork. Don't be afraid to ask questions. The seller should have nothing to hide.

AFTERMARKET SUPPLIERS OFFER A RANGE OF PERFORMANCE PARTS, MEANING YOU DON'T NEED TO DIG DEEP AT YOUR LOCAL MAIN DEALER



BODYWORK

"We've yet to see a 987 Cayman with heavy bodywork corrosion roll through Hartech's workshop doors," muses Grant. "The metalwork on these cars was fully galvanised at the factory. Rust is rare, but it's worth keeping an eye out for signs of accident repairs, especially on S-badged 987 Caymans, which are likely to have experienced far more in the way of 'spirited' driving than the 2.7-litre model."

Check to make sure panel gaps are as they should be. Look for differences in colour between sections of bodywork. Invest a couple of quid in an online history check from mycarcheck.com. You'll get instant confirmation of any registered accident history (insurance claims), as well as details relating to changes of registration, any outstanding finance

and the number of previous keepers. You'll also be able to tell if the 987 Cayman you're eyeballing was ever stolen or subjected to a change of colour during its lifetime.

Kneel down at the front of the car and check the condition of the radiators. They're positioned directly in the line of fire of stones and other road fallout, and can easily be compromised through impact damage. A previous owner may have fitted grilles over the intakes as a preventative measure.

Headlamps can discolour. They can also attract surface imperfections through stone chips. Replacement lights are expensive, so consider purchasing a headlamp restoration kit as your first attempt to rectify the problem. Meguiar's and Autoglym produce kits available from Halfords.



BEATING THE BEST

A 987 Cayman prepared by privateers finished fourth overall in the 2007 24 Hours of Nürburgring. The car beat a pair of 997 GT3 RSRs, a 997 GT3 Cup and a 996 GT3 Cup.



BRAKES AND SUSPENSION

While Porsches enjoy being used regularly, adding miles contributes to the wear of steering, suspension and braking components. Caymans used at the track are especially prone to deterioration in these areas, so check the car's history file to see when parts were last replaced.

You're unlikely to feel much of a difference by fitting one or two new bushes, so consider investing in a complete suspension kit, including fresh dampers, springs, bushes and professional four-wheel laser alignment. This might sound like overkill, but shock absorbers and bushes celebrating double-digit birthdays simply won't be performing at their best. You want your Porsche to feel tight in the twisties, don't you?! Consider the cost of some or all of this kit in the price you're prepared to pay for your new 987 Cayman.

Aftermarket suppliers offer a range of performance parts ensuring you don't have to dig deep at your local main dealer. Bilstein, GAZ Shocks and Öhlins offer various damper solutions, while polyurethane bushes from Powerflex, springs from H&R or Eibach and braking equipment, including pads in a wide range of compounds to suit different driving styles, are available direct from EBC Brakes.





INTERIOR

987 Boxsters and Caymans, along with 997-generation 911s, feature cabin furniture far superior in quality to 986 or 996 interiors. Fantastic build quality means even high mileage Cayman cockpits should be in good condition. The leather on seat bolsters can wear where drivers drag their butts, and you might find in-car entertainment and satellite navigation functionality far more dated than you'd like, but these are minor gripes, the latter easily remedied by appointing an aftermarket head unit. "Gearbox preferences aside, my advice would be to buy based on overall condition, not on specification," says Grant. "Don't miss out on the opportunity to buy a brilliant car because you fancy a different stereo system!"



TALKING SHOP

A fully optioned early 987 Cayman is a rare thing thanks to few original buyers shelling out for PASM, ceramic brakes, rear parking sensors, heated seats, cruise control, PCM2, Sport Chrono or Xenons.



THE CAR'S THE STAR

The 2006 987 Cayman S featured in this article was supplied by Chester-based performance vehicle specialist, MW Performance, and is fitted with H&R lowering springs, a DesignTek exhaust, an IPD plenum, a larger intake pipe, a free-flowing air filter and a larger-than-stock throttle body. "The new owner intends to use the car as his track toy," confirms MWP boss, Marc Wale. "The Cayman's near equal front-to-back weight distribution and low profile make it an ideal choice for anyone wanting to attack the asphalt without spending the kind of money buying a similarly aged 997 will empty your wallet for," he adds.

Grant echoes Marc's sentiment. "You can buy a 987 Cayman S for a significantly lower purchase price than a 997, yet Porsche used many of the same parts across both models. Coupled with the 987's fantastic levels of performance, this fact makes the Cayman the better value car. All you need to do is decide whether you can live with a Porsche possessing only two seats!"



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LIFE BEGINS AT FORTY

We popped along to Three Sisters Circuit to join the 924 Owners Club in celebrating four decades of the earliest turbocharged transaxle.

Words & Photography **Dan Furr**

As demonstrated throughout Porsche's seventieth birthday celebrations last year, nothing excites enthusiasts of a particular make or model of car quite like an anniversary. A significant milestone reached offers the perfect opportunity to party in style, which is exactly what members of the excellent 924 Owners Club (porsche924.co.uk) did by paying tribute to the 924 Turbo's fortieth trip around the sun just before we went to print with this issue of *GT Porsche*.

Three Sisters Circuit in Wigan played host to the big birthday bash, a venue hired by the club in order for its members to experience track time in a safe, friendly environment packed with tip-top transaxles and their dedicated owners. Pristine examples of first and second-generation Turbos were on display, including the award-winning twin-tone Series 2 originally owned by Colonel Gaddafi's personal pilot, Neville Atkinson.

Genuine and replica Carrera CTs were proudly exhibited alongside standard and heavily modified 924s loaded with forced induction. One of the wilder cars in attendance was the 924 DP Cargo driven to the event by Gmund Cars head honcho, Andrew Mearns. The distinctively styled shooting brake is kitted-out with the guts of an early 924 Turbo and fascinated those who took the opportunity to view its custom coachwork up close, although it's fair to say the super-rare retro ride was competing for attention alongside

a wide variety of modified water-cooled wonders, not least of all John Blackwell's supercharged, Lexus-coloured silver stunner.

For the benefit of those wanting to hit the venue's tight, twisty track, warm weather and bright sunshine ensured dry driving conditions throughout the day. Novices could take advantage of expert driver tuition and thrilling passenger laps courtesy of guest instructors, including 2016 BRSCC Toyo Tires Porsche 924 Championship winner, Linda Warren. Her successor, 2017 champion, Pip Hammond, was at the event with his partner, Esther Quintmere, and the pair's naturally aspirated two-litre 924. Yep, despite focus firmly on Turbos, all 924s and their proud pilots were invited to take part in the day's activities, resulting a high number of early transaxles spanning all ages and specification arriving from towns and cities far and wide.

Awards were dished out for furthest travelled (seven hours), car of the show (Gaddafi's mate's Turbo) and to those who have contributed to the club above and beyond their role as regular members. Club Publications Manager, Pete Saysell, was crowned recipient of the Relentless Excellence award in recognition of his work on the celebrated club magazine, *TwoFour*, not to mention his efforts to raise the club's profile across multiple social media platforms. A raffle raised much needed funds for the Campaign Against Living Miserably (CALM), a leading charity working hard to prevent male suicide in the UK.



"We achieved great success with our 92Forty event at Silverstone to commemorate the 924's fortieth year in 2016," recalled Club Events Manager, Ed Myland. "Hosting the 924 Turbo's birthday celebrations at Three Sisters Circuit was an excellent way of attracting a greater number of owners living in the north of Britain. Additionally, the venue's track is perfect for exploring the 924's abilities away from wider, more crowded circuits found elsewhere in the UK." We're looking forward to the Turbo's fiftieth birthday bash already! ■





MODEL BEHAVIOUR

The 924 Turbo was introduced to Porsche fans in 1979 as a way of bridging the gap between the 911 and the entry-level 924. More than 11,600 examples were manufactured.



Left Mark Hayes wasn't shy when it came to giving his genuine 924 Carrera GT stick around the track

Facing page Transaxle spares specialist, Paul 'Woolie' Woollard was on hand to offer parts and advice to those in attendance



SERIES 2 924 TURBO

The club award for 924 Turbo of the show went to Pete Gresty's 1981 twin-tone Series 2, an immaculate low miler complete with luxurious chocolate brown leather and its original cost option Blaupunkt cassette player. "The car belonged to Neville Atkinson, Colonel Gaddafi's personal pilot between 1972 and 1982," explained Pete Saysell, who drove the car to Three Sisters Circuit in his namesake's absence. Atkinson served in the Fleet Air Arm of the Royal Navy before serving his Libyan paymaster, a job which saw him fly government ministers and various world dignitaries around the Middle East and Africa. A year before his time with Gaddafi came to end, Atkinson bought this gorgeous green Turbo, a C16 right-hand drive model. The car was shipped back to the UK in 1983, hence the Y-plate it wears today. Immaculate in every way, it came as little surprise to see this special 924 take top honours in what was a hard-fought contest. Many congratulations, Mr Gresty!

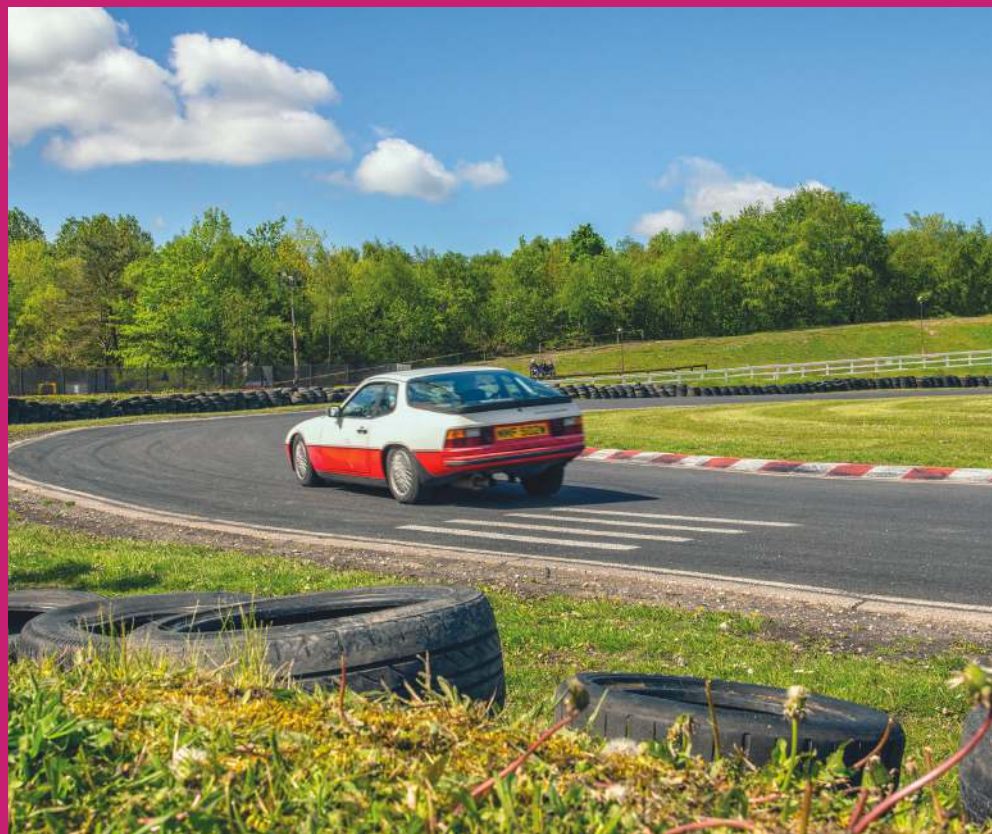


924 DP CARGO

Andrew Mearns is a name many *GT Porsche* readers will be familiar with due to his profile as owner of classic Porsche sales specialist, Gmund Cars (gmundcars.com). "My 924 DP Cargo is one of three examples produced in the early 1980s," he tells us. "It's believed to be the very first 924 converted to shooting brake specification by German Porsche modifier, DP Motorsport, and stands apart from the company's later two 924 Cargo builds thanks to the fact it retains narrow bodywork instead of adopting wider 944 quarters." An early 924 Turbo drivetrain propels the peculiar load lugger, which is based on a 1976 924 and features an altered Volkswagen Passat roof. "I bought the car four years ago after the daughter of the original buyer contacted me from where she lives in Sweden," reveals Andrew. "She told me she'd been using her deceased father's 924 DP Cargo to commute to and from her local supermarket and wondered if I'd be interested in buying the car. When somebody offers you a rare and unusual Porsche, you don't say no!"



A RAFFLE RAISED MUCH NEEDED FUNDS FOR A LEADING CHARITY WORKING HARD TO PREVENT MALE SUICIDE





Left Porsche Centre Bolton came along to the event with a new 718 Boxster T



Facing page
2016 Porsche 924 Championship winner, Linda Warren, in her natural habitat

SUPERCHARGED 924

John Blackwell's supercharged transaxle started life as a standard two-litre 924 first registered in 1983. That was long before he set to work transforming it into the 300bhp belter it is today. "I've owned the car for more than sixteen years," he beams. "I fitted a 944 front bumper, lowered the ride height and added aftermarket wheels, but I couldn't escape the feeling I was driving a sheep in wolf's clothing." The orthopaedic surgeon's frustration was made worse when his wife bought a turbocharged Audi TT and his brother was enjoying seat time in a Mazda MX-5 equipped with a supercharger. "I bought a wrecked Turbo and planned to transfer its innards into my silver 924, but plans changed after I decided to fit an Eaton M62 supercharger to the Turbo's engine. Wind the clock forward, and the car is now producing big bhp managed by an Adaptronic ECU supported by the appointment of an Aston Martin DB7 blower, a BMW 328 throttle body, MX-5 coil packs, Subaru Impreza WRX fuel injectors, a Ford Mondeo fuel rail, a BMW Mini Cooper S bypass valve, a SAAB 900 Turbo idle control valve and a Citroen Picasso air filter. A 944 Turbo S gearbox kitted-out with a limited-slip differential benefits from a 911 SC clutch, while swift stopping is taken care of by Jaguar XKR anchors. "I've added Spax coilovers, 968 anti-roll bars and exterior decoration from the same model," grins John, going on to point out the subtle difference between his car's original coat of OEM Zermatt Silver and Lexus Platinum Ice, the shade it currently wears following a glass-out respray.





Left Club events guru, Ed Myland, looking suitably pleased with the day's proceedings



Right A superb selection of naturally aspirated 924s were on display alongside the turbocharged transaxles being celebrated at Three Sisters Circuit

924 CARRERA GT

One of the most desirable of all 924s is the iconic Carrera GT. Mark Hayes acquired his Guards Red road rocket from Germany after time spent owning a faithful replica. "My genuine Carrera GT was painted white by its previous owner," he remembers. A stint in a spray booth soon followed the car's arrival at Mark's home in North Wales, where radiant red was rightly reinstated. Despite the wide-arched 924's rarity, he's not afraid to put the left-hooker through its paces, as demonstrated by the numerous high-speed laps he was seen completing at Three Sisters Circuit, plus his admission the car carried him all the way to the Le Mans Classic last year. "It was the perfect Porsche for the trip. Powerful, practical and comfortable!" What more could you want from Stuttgart-crested sports metal?!



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ON THE AIR

The most commonly purchased aftermarket air filter designs and how effective they are at balancing the rate of filtration with derestricted airflow

An air filter is always the first upgrade made when an enthusiast looks to improve their car's performance, but this key alteration can quickly be overshadowed by more exotic modifications. This is unfair. After all, the humble air filter is a crucial component in the quest for increased horsepower. Moreover, an engine without an air filter in place will most likely destroy itself through the inhalation of dirt and debris. In the worst-case scenario, your Porsche's beating heart would shotblast itself to death from the inside out!

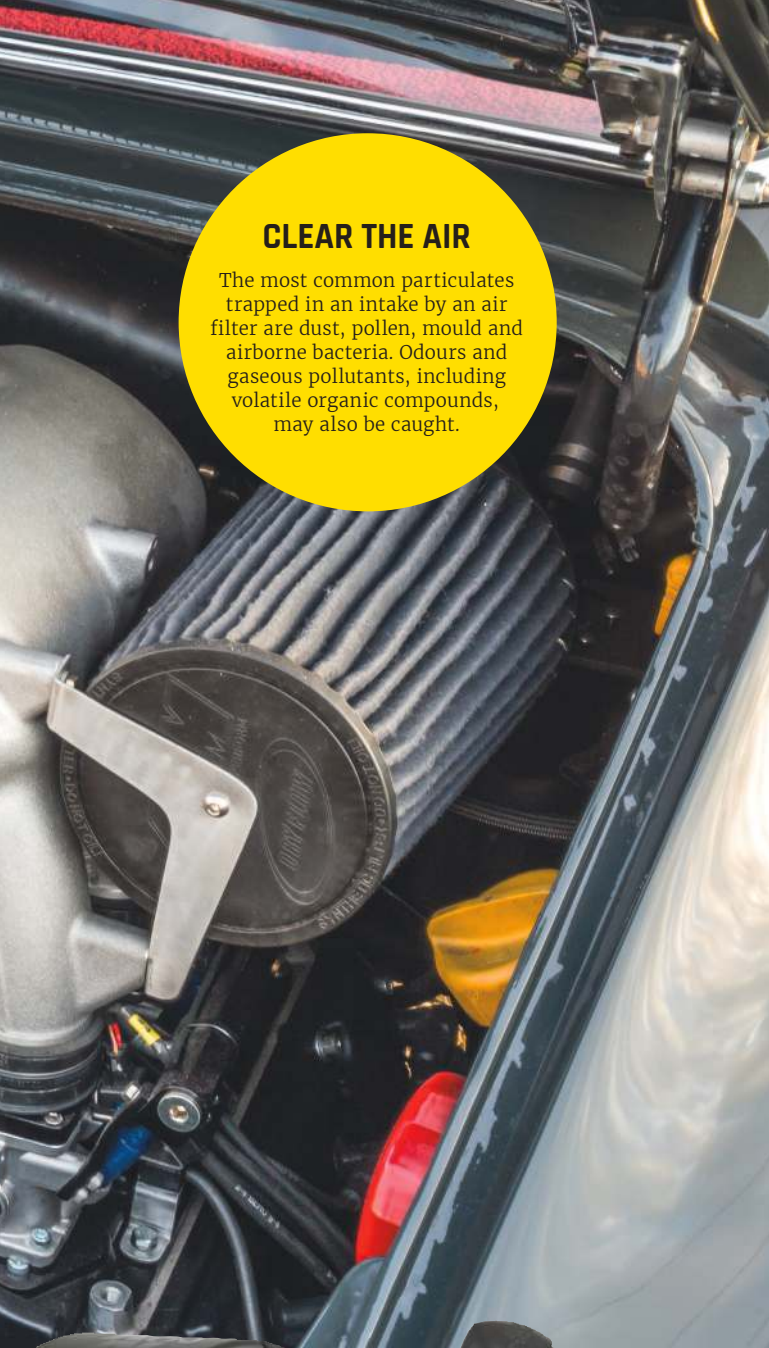
By its nature, an air filter introduces a restriction of some kind

or another, meaning choosing the right air filter can be tricky. The key to delivering optimised airflow suited to a specific application is to reach the perfect compromise between levels of filtration, airflow and intake air temperature. Additionally, the resulting air filter needs to fit into the space available in the host vehicle's engine bay, which is often tightly packed.

As a rule of thumb, a filter with high filtration properties will flow less air. In other words, you'll need a larger filter to flow the same volume of air as a filter with lower filtration properties. The temperature of the intake air is just as important; hot air is less dense than cold air, meaning there's more oxygen available for

CLEAR THE AIR

The most common particulates trapped in an intake by an air filter are dust, pollen, mould and airborne bacteria. Odours and gaseous pollutants, including volatile organic compounds, may also be caught.



combustion in cold air. This is why temperature plays such an important role when it comes to induction and achieving big bhp, and why a rolling road operator has to make calculations which consider the effects of volatile ambient temperature before he or she is able to provide you with an accurate power figure after your Porsche has enjoyed a session on the dyno.

There are many types of aftermarket air filter available for you to choose from. A variety of different materials can be used depending on the desired filtration rate and intended application. In this article, we take a closer look at the most popular designs, examining how each of them works.



FILTRATION VS FLOW

With any air filter, there's a trade-off between levels of filtration and the volume of airflow. Put it this way, if you simply blanked off your car's intake pipework, you'd stop all dirt and debris from entering the engine, but you'd also stop air from being sucked in, meaning the engine wouldn't run!

At the other end of the spectrum, an open intake with no filter whatsoever would pose zero restriction to the amount of air which could enter your Porsche's powerplant. All dirt and debris would also be sucked in, causing serious damage.

Clearly, we need a compromise between these two extremes. Most standard air filters tend to fall nearer the 'closed' example we've just outlined, ensuring high levels of filtration for maximum

engine protection and increased service life. All of this sounds great, but it comes at the expense of airflow, and as we already know, a higher intake of air means more power is generated.

Thankfully, aftermarket performance air filters allow you to shift the rate air can enter your car's engine, leaning towards the 'open' example we've just highlighted. The increased flow rate allows you to unlock more of your Porsche's true performance potential. In theory, this should come at the expense of filtration, but by deploying clever tricks of the trade (uprated materials, custom filter shapes and a choice of sizes), huge improvements in airflow can be achieved whilst maintaining excellent levels of filtration. Turn the page to read about different filter materials.



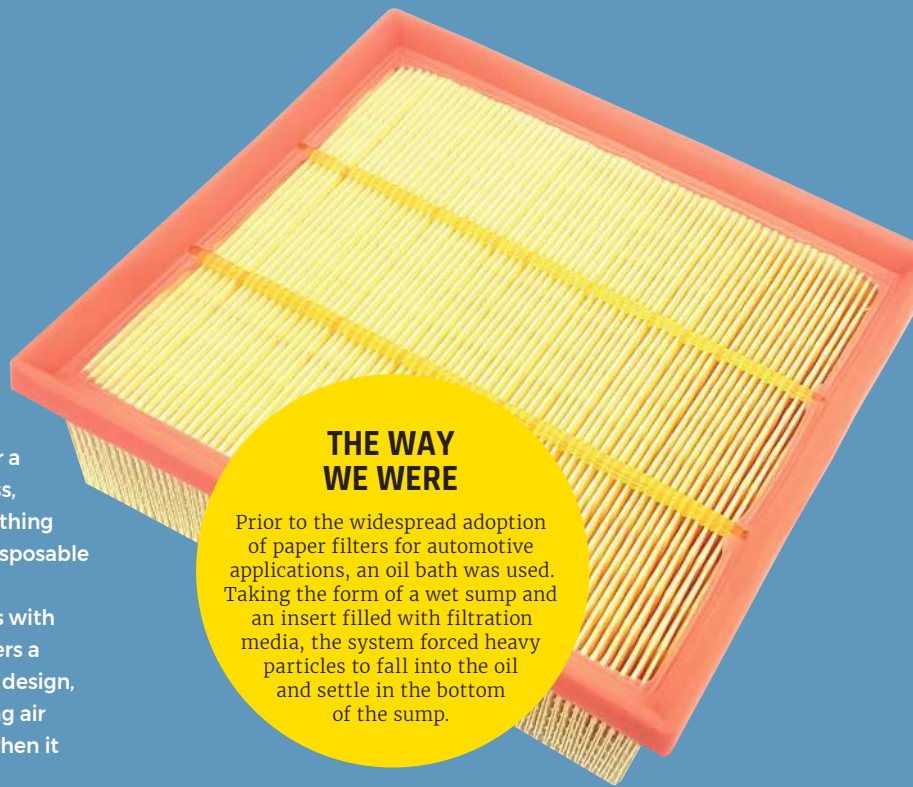
MATERIALS

There are many materials which can be used to create a performance air filter, but the four most commonly used properties are paper, cotton gauze, foam and mesh. Let's look at each of them in detail.

PAPER

Most standard air filters are made from paper. Paper is very good at filtering dirt and debris, but as the filter does its job, it becomes dirty and begins to restrict airflow. Imagine looking at a paper element under a microscope. You'd see lots of tiny holes allowing air to pass, but these same holes get blocked quickly, preventing anything – even air – from passing. This is why paper air filters are disposable and have a short service life.

As outlined here, a paper filter's performance decreases with age. To increase service life, manufacturers give paper filters a large surface area, which explains their distinctive fin-like design, but even when new, paper filters aren't as good at allowing air to pass as the other filter types and will pose a problem when it comes to tuning your Porsche's engine.



THE WAY WE WERE

Prior to the widespread adoption of paper filters for automotive applications, an oil bath was used. Taking the form of a wet sump and an insert filled with filtration media, the system forced heavy particles to fall into the oil and settle in the bottom of the sump.



TO INCREASE SERVICE LIFE, MANUFACTURERS GIVE PAPER AIR FILTERS A LARGE SURFACE AREA, WHICH EXPLAINS THEIR DISTINCTIVE FIN-LIKE DESIGN

COTTON GAUZE

Cotton gauze allows for higher airflow than paper. Typically, a cotton gauze air filter will make use of four-ply gauze with a specially designed aluminium screen. The metal offers support to the cotton gauze and allows the filter to be corrugated across its surface area. A large surface area has many benefits over a smaller one, but one of the main advantages is a marked increase in the filter's service life.

Air filters made from cotton gauze use a specially formulated oil to assist with filtration. When contaminated air passes through the filter, any dirt present gets trapped in the oil. This vastly increases



the air filter's ability to stop unwanted particles entering your Porsche's engine without reducing the volume of air being sucked in.

Cotton gauze filters can limit the movement of particles as small as five microns. Anything smaller is unlikely to cause harm unless being drawn into the engine in large quantities. To put this into perspective, most standard paper air filters are unable



to filter particles smaller than twenty microns!

Cotton gauze filters are designed to be cleaned and reused. Simply ridding the filter of trapped dirt will restore the part's performance back to its as-new state. Indeed, the guys at K&N Filters are so confident in the quality and durability of the company's famously pink cotton gauze parts, they offer a million-mile warranty with each air filter they manufacture!



FOAM

Foam air filters are used in a wide range of applications, including top tier motorsport, such as the British Touring Car Championship, the World Rally Championship and Formula One. Foam filters differ from cotton gauze and paper filters due to their nature as a 'surface' medium. In other words, they block dirt and debris on the filter surface before allowing air to pass.

Foam filters are said to offer a high rate of filtration due to their thickness. Dirt and debris gets caught in the filter, but there's always an alternative route for air to flow freely. The labyrinth-like fashion of foam air filters is why they tend to have a lengthy service life and maintain their performance far longer than other surface media air filters.

Another advantage is the ability to laminate different grades of foam to produce a filter with unique characteristics. For example, take a layer of coarse foam and put it atop a layer of finer foam. Before you know it, you've produced an air filter with very little restriction through the first layer, though larger particles will continue to be trapped. Then, as the air flows through the second layer, an increased volume of dirt is blocked from entering the engine, although the air will be more restricted than when it made its way through the first layer. You could add a third layer that's finer still, which would trap the smallest of particles, albeit at the expense of airflow.

As you can see, laminating filter foam like this allows almost infinite scope for adjustability, enabling a bespoke filter to be produced for a specific application. As if to prove the point, World Rally Championship cars change foam air filters from stage to stage, primarily because most rally stages deliver dramatically different weather and surface environments from the stage before. This constant change of conditions requires a variety of air filter properties to be called upon.

Foam air filters use a specially designed oil to help trap dirt and debris, blocking particles as small as four microns! Like cotton gauze filters, foam air filters can be cleaned, re-oiled and reused, with filter manufacturers offering the necessary cleaning materials at extra cost. Some manufacturers, including Pipercross, even offer foam filters mated to carbon-fibre intake pipework.



CABIN FEVER

A fairly recent introduction to production cars is the cabin filter. Typically a pleated paper air filter, it can be replaced with an aftermarket performance panel filter, just like an engine intake filter.

MESH

Mesh filters aren't all that popular on the Porsche scene, but they are often seen strapped to the powerplants of Japanese joy toys. The principle behind how they work is the same as paper and cotton gauze filters: the layers of fine stainless steel mesh act as a filter which helps to prevent dirt and debris from entering the engine. The consensus among modifiers, however, is that because of the way stainless mesh is constructed, this type of air filter fails to offer the same rate of filtration as paper, cotton gauze or foam. The holes within the mesh allow vastly improved airflow over the host vehicle's standard paper air filter, which is why you should expect to see good power gains with a mesh filter



in place, but don't expect the same level of filtration as you'd enjoy after investing in an air filter made from any of the other materials we've talked about in this article. If outright power (at the risk of engine durability) is your thing, then a mesh filter may suit your car and the way you drive it. If not, consider a safer form of filtration.



SIZE MATTERS

When it comes to the wonderful world of air filters, bigger is better. That said, you shouldn't make the mistake of thinking a bigger filter will automatically deliver enhanced performance. Each application has its own optimum filter size. Exceeding it won't make any difference to the power your car's engine is able to produce. Then again, most people fit the largest air filter they can squeeze into the available space beneath the bonnet of their car to ensure they aren't restricting airflow. Also, a larger air filter features a larger surface area, which means it should have a longer service life before cleaning is required.

In theory, fitting a filter larger than what's necessary won't cause any harm, but there is increased danger of the filter's larger dimensions causing the part to edge closer to heat generating componentry, and the last thing we want is for hot air to be sucked into the engine. Our advice is to fit an air filter designed for your intended application, or take measures to prevent a larger filter from being unduly exposed to heat.

Fitting an air filter that's too small will cause problems. Essentially, the filter becomes a restriction, even if made from high-flowing performance materials. The engine will be choked of air, robbed of performance and may suffer damage as a consequence of being subjected to prolonged 'breathing difficulties'.



**DON'T MAKE
THE MISTAKE
OF THINKING A
BIGGER FILTER WILL
AUTOMATICALLY
DELIVER ENHANCED
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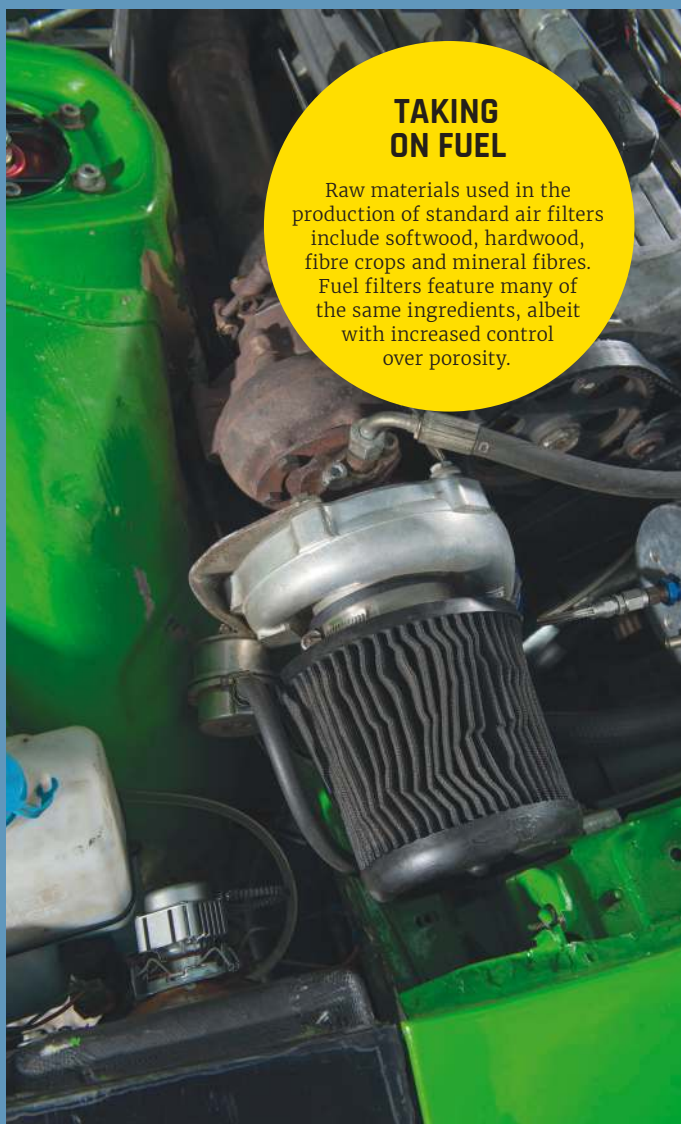
PANEL FILTERS

Aftermarket panel air filters are seen as the first stage of upgrade in the world of tuning. As we've already discussed, replacing your car's standard paper air filter with a freer-flowing panel filter made from cotton gauze or foam allows more air to reach the engine. This means it can put less effort into pulling air through the filter and can suck more air in, resulting in more power.

A performance aftermarket panel filter has many advantages over the standard specification part it replaces. For a start, a performance panel filter will, in most applications, flow forty percent more air. Also, this type of filter benefits from being a direct replacement for the standard item, meaning

quick and easy installation taking no longer than you'd spend fitting a regular paper filter. And, of course, a performance filter has a much longer service life.

Don't be fooled into thinking an aftermarket performance panel filter will always improve performance. Much depends on the size of the air box in which the air filter is housed; if the air filter is capable of flowing more air than the airbox design will allow, it doesn't matter how good the flow rate of your new air filter is, there will always be a limit to its ability to release trapped ponies. Under these circumstances, you'll need to look at replacing the stock air box with an aftermarket part or induction kit.



TAKING ON FUEL

Raw materials used in the production of standard air filters include softwood, hardwood, fibre crops and mineral fibres. Fuel filters feature many of the same ingredients, albeit with increased control over porosity.

CONE/CYLINDER FILTERS

One of the most popular aftermarket performance air filters is a cone or cylinder filter. The benefit of a cone over a flat panel is simply that a cone offers a greater surface area whilst still fitting within a small physical space, often narrower than that occupied by the panel filter. Think of it like this: rolling a panel filter into a cone doesn't change the size of panel's surface area, but it does allow the part to squeeze into a small space.

Installing a cone or cylinder filter often requires the removal of the car's original air box. The new filter is then installed directly onto the end of the intake trunking. Alternatively, it can sit within a bespoke air box or behind a custom heatshield. This setup is usually what people refer to as an induction kit. It eliminates the standard air box's restrictions by drawing air in from all directions instead of relying on air being fed inward via a specific route dictated by uncompromising air box ducting.

The downside to all of this is that controlling where intake air is drawn from can prove to be a challenge, with some poorly designed filters pulling in hot air from around the engine bay. This is what's referred to as 'heat soak' and is massively detrimental to performance. If you're really unlucky, your car might be producing far less power than it was with the stock air filter in place!

Open air filters work well on naturally aspirated engines where improvements in airflow feel more pronounced than when playing with air filters linked to a turbocharger or supercharger. Moreover, tuned naturally aspirated engines need large volumes of air to be drawn from the atmosphere, but often feature air boxes which won't allow air to flow beyond the manufacturer's standard specification, which is likely to be super-restrictive.

Cone filters used in forced induction applications tend to be much larger than those fitted to naturally aspirated engines. This is simply because forced induction applications usually produce more power than those free of a turbocharger or supercharger, hence the need for a greater volume of intake air.

THROTTLE BODIES

When making use of throttle bodies, you've got two main filter choices: smaller individual air filters (one for each throttle body) or a giant filter which covers all inlet trumpets at once. The filter you choose depends on the application and its requirements. Additionally, limited space in your Porsche's engine bay may prove to be a key factor preventing you from fitting a large single air filter with a superior flow rate.



COLD AIR INTAKE SYSTEMS

As we've already mentioned, hot air isn't as useful to an engine as cold air. Shielding the incoming air from sources of heat, such as turbochargers and exhaust pipework, will always result in better performance. One way of avoiding the dreaded 'heat soak' whilst maintaining an open filter's rate of airflow is to enclose the filter in a specially designed casing that can only receive air from a dedicated cold air feed. Granted, it sounds like we're describing the standard manufacturer airbox setup, but we're talking about a completely derestricted air filter housing suited to your car and the power you hope it will achieve.

It's true to say many modern sports cars feature well-designed air boxes with multiple inlets and excellent heat shielding, but even good OEM designs can be improved upon. Besides, most older vehicles are fitted with terribly designed air boxes which massively restrict engine performance.

One of the simplest ways to combat 'heat soak' is to take a cone air filter and fit it inside a carbon-fibre, plastic or aluminium air box. You can then direct cold air through strategically positioned ducting (on a transaxle, consider losing a fog lamp to make way for the intake opening). It's a method which yields good results, but unless you're building a bespoke air box, don't expect an off-the-shelf aftermarket part to fit your Porsche's engine bay without fettling. Air boxes can be cumbersome and awkward to install!

Fortunately, many tuners will be able to recommend performance air boxes with optimised pipework design. They'll also be able to provide you with information relating to the best choice of air filter for your chosen Porsche. Naturally, bespoke solutions often attract a premium price dictated by the choice of materials and complexity of design deployed. Rest assured, if you're shelling out for a custom design, you'll almost certainly be investing in the best airflow solution available for your sports car.



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Which Porsche celebrates its fiftieth anniversary this year?

- a) 914
- b) 924
- c) 964

Head over to mediajustice.co.uk/gtp and register your answer. It really is that simple! The competition closes Friday 12th July. We'll announce the winner in a forthcoming issue of the magazine.

CONTACT

For further information about the supported features of Road Angel PURE, plus details of the company's other road safety products, visit roadangelgroup.com or call 0330 320 3030.

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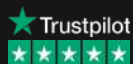
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930 SE



BEST WISHES

Already one of a rare breed, this factory-built, second-generation 930 SE Flachbau features unique specification and packs a 355bhp punch

Words **Dan Furr** Photography **Dan Sherwood**



Popping to the shops later today? Chances are, you'll return home with items you didn't intend to drop into your basket. "Accidental purchase," smiles David Watson, speaking from experience. He's not referring to the act of sliding a cheeky packet of chocolate biscuits past the checkout, though. No, the financial software services specialist concluded a spot of retail therapy in 2006 with an altogether more substantial unplanned purchase: a paint-to-sample Ferrari Rosso Corsa 930 SE Flachbau!

In the last issue of *GT Porsche*, we presented a 930 converted to flat-nose specification by Dage Design many moons ago, but what you're looking at here is the real deal. Furthermore, David's whale-tailed 911 is one of what's thought to be only 204 second-generation Flachbaus delivered by Porsche's Sonderwunsch (Special Wishes) department between 1983 and 1987. The first batch – numbering fifty-eight units – featured 'hammerhead' spot lamps integrated into the front bumper. These cars also wore smoothed wings, meaning the model's later pop-up headlamps were absent. From 1987, the 930 SE became a Porsche production model, available on dealer price lists with the M505 (US) and M506 (rest of world) option codes, but the four-year period when David's factory Flachbau was assembled required buyers to make direct contact with Porsche to request a 930 SE build to custom specification.

The SE accounts for a tiny percentage of total 911 Turbo production volume, making even the late-spec 330bhp Turbo S-based builds a rarity. David's Flachbau's origins as a second-gen Special Wishes order ensure membership of an even more exclusive club, and that's before you begin to spot differences between his car and those equipped with typical SE styling intended to ape the look of the legendary

935 race cars. The boxy side skirts, for example, are nowhere to be seen. The imposing vents louvred directly above the pop-up lamps are missing. And then there's the Maranello paintwork. Rare? This is a one-off!

DESERT CHERRY

How exactly did David end up heading out for milk and returning with a unique Porsche? Well, the milk in question was actually a Carrera 3.2 Club Sport. "I owned a standard Carrera 3.2 at the time," he tells us. "I was considering the purchase of the Club Sport when I spotted this Flachbau sitting in the corner of the dealer's showroom. The car was in need of work as a consequence of being used as a daily driver in London and Colchester for many years," he adds, confirming the wide-arched left-hooker was always kept on the street following its import from Saudi Arabia in 1997. Unable to resist the exaggerated profile and unusual looks of the SE he was staring at, our man became its second UK owner a short while later.

When new, the car was supplied by the Samaco Bin Laden Company, Porsche's official dealer in Jeddah. Boxes ticked include the unusual bodywork options, a limited-slip differential, colour-to-sample magnolia perforated hide, a leather headlining, a walnut dash, cassette dropdown bins and a centre console originally featuring a telephone keypad. Sadly, this feature has long been removed, unlike the gold-plated gear knob and the brass Porsche crest at the centre of the car's Special Wishes steering wheel.

Like many SEs produced for markets outside the United States, David's Red Sea road racer was treated to engine upgrades raising power above the M505 specification of 330bhp. He's chasing a fascinating paper trail to determine whether the work was carried out by Porsche at the

Facing page
The option of front wing vents and side skirts was dismissed by the original buyer in Saudi Arabia

Below David is unsure whether the engine upgrades were carried out at the factory



NOSE TO TAIL

948 factory 930 SEs were produced between 1981 and 1989. The vast majority of those built were third-gen M505 Flachbaus for the US domestic market.

WHEN NEW, THE CAR WAS
SUPPLIED BY THE SAMACO BIN
LADEN COMPANY, PORSCHE'S
OFFICIAL DEALER IN JEDDAH





HAMMER TIME

The prototype factory Flachsbau was built at huge expense in 1980. The car featured the 935-aping 'hammerhead' front bumper and integrated light clusters.





Above It's a handful around corners, but chuck the car in a straight line, plant the throttle and wait for boost to kick in for uncontrollable grinning to begin

point of sale or was a separate commission in Saudi Arabia at a later date. What he can say with confidence is that his car is producing a dyno-tested 355bhp on minimum boost. "When compared to my Carrera 3.2, the SE is almost refined in nature," he says. "The 3.2 was kitted-out with firm sports suspension and a TechArt exhaust, which was great fun, but a little too raucous at times. The SE's turbocharger keeps the flat-six engine tone far more civilised, although I'd be lying if I said I didn't enjoy the pops and cackles heard at the back of the car on overrun!"

The surge of power from 3,000rpm all the way to the redline is addictive, even if you have to plan ahead due to comical lag. The car carries more weight into corners than a Carrera 3.2, meaning it isn't as nimble, but the sensation of heading for the horizon on full chat is unlike any modern Turbo experience, and goes a long way toward explaining why David uses the car for long distance continent cruising. Indeed, some of you may recognise the turbocharged 911 from its numerous visits to the Le Mans Classic, including a starring role on The Independent Porsche Enthusiasts Club (TIPEC)'s show stand. Additionally, David has let rip on the Nürburgring's hallowed asphalt, yet his time playing on a track hasn't always been entirely successful. "I managed to lurch the limited-slip differential's clutch plates at Spa," he sighs. Needless to say, the unit has been completely rebuilt, as has the turbocharger and braking system. Injector seals have been replaced for good measure, too.

ONE LOUDER

Regular servicing has been taken care of by Unit Eleven Porsche Specialists in the North East of England, ensuring David's bespoke 930 remains in tip-top mechanical

condition at all times. Not that he's comfortable putting the car to use as often as the previous owner. "Escalating values of classic 911s means I have to think twice about where I take or park my SE," he groans. "Any Ferrari-red sports car attracts attention, let alone with looks as evocative as a Flachbau." That said, he's added a strong 26k miles to the odometer since buying the car thirteen years ago, proving it continues to get regular use. "I'll be taking it to the Silverstone Classic in July," he reveals. Any more track time booked? "Probably. Prior to killing the diff, time on the circuit in Belgium was an experience prompting a huge grin whenever the turbocharger spooled in anger. Unfortunately, the car wasn't too clever in corners, where the modern Italian machinery I was matching on straights took no prisoners when rapidly downshifting around bends. It became clear to me that while my Turbo can hold its own at a track, I need more seat time to learn how to get the best out of 1980s forced induction technology."

His enthusiasm for the Porsche brand extends beyond classic 911s. A revolving cast of 928s, including a Grand Prix White S4 daily driver, has featured on his motoring CV, as has a 924S. Today, a 1.8-litre 914 shares garage space with his fantastic Flachbau. The mid-engined marvel was imported by David from California before a three-year restoration with help from members of Porsche Club GB's excellent 914 register. There's a rare Minerva Blue 928 4.5 (one of only 148 non-S UK cars with manual transmission) trimmed in Pascha cloth keeping him occupied, plus he's putting the finishing touches to 911flachbau.com, his recently launched website acting as a useful resource to anyone considering investing in a 930 SE of their own. Whether they set out to buy one when leaving the house, of course, is another matter entirely.

Facing page
Luxurious magnolia leather is joined by a brass Porsche crest and a gold-plated gear knob

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FIVE MINUTES WITH CHRIS LANSBURY

This month, we take time out to chat with the man behind one of the UK's fastest-growing independent Porsche specialists, PIE Performance. And in case you're wondering, PIE stands for Porsche Independent Engineering!



Who are you and what do you do?

I'm Chris Lansbury, Managing Director at PIE Performance.

When was the business established?

PIE Performance has been trading for over a decade, but the company can trace its roots back to the shed I was using during the restoration of a classic 911 fifteen years ago. I was buying donor cars for components I needed to complete my project, selling items that were supplementary to requirements. Before long, I'd amassed a huge number of spares. Recognising a need for good quality second-hand Porsche kit, I started acquiring 924s, 944s and 928s with the intention of harvesting them for parts I could sell on.

Do you still offer used transaxle parts?

No. The business grew and evolved rapidly, resulting in today's ten-strong team of PIE

Performance employees covering all aspects of professional maintenance, restoration, tuning and sales of Porsche cars, from the 356 to the very latest models. Occupying workspace in Suffolk, we pride ourselves on the quality control aspect of the work we do, with every job being subjected to strict checks before the car being worked on leaves the PIE Performance workshop. The same applies to the vehicles in our sales showroom, a space currently occupied by a 944 drop-top, a 996 Turbo, a 924S, a 964 Turbo, a low mileage 930, plus newer Porsches, including a 986 Boxster 2.7 and a 997 Carrera.

Tell us about your motorsport ambitions.

Following a successful season racing in the BRSCC Toyo Tires Porsche Championship with motorsport driving instructor, Toby Smales, campaigning our specially prepared Boxster, we're looking forward to returning

to the series later this year. Building a race car and supporting Toby trackside is a great project for the PIE Performance team to be involved with, and an opportunity for the company's technicians to put their skills to good use away from the workshop.

You're not competing in a full season of the championship, then?

Demand from the customer base has seen the business grow at fast pace. A requirement to expand means we need to move into new premises with enough room for extra ramps and more technicians. Continually reinvesting in diagnostic and tooling equipment in order to support the PIE Performance workforce is our priority, which is why we won't be returning to action with the Boxster until we're settled in a new workshop.

What was your first Porsche?

A Pastel Beige Carrera 3.2.

What's your favourite Porsche?

The 968 Clubsport I owned a few years back.

The best thing about Porsche ownership?

No other manufacturer produces cars that'll make you smile the way a Porsche does!

Can we follow your adventures online?

Visit pieperformance.co.uk, where you can read about what we do, the history of the business and the cars we're working on.



PORSCHE WINS EARLY WEC TITLE

Adverse weather fails to prevent victory at penultimate round



Porsche is the new FIA World Endurance Championship (WEC) winner, a title bagged at the penultimate round of the series. The 911 RSRs won the manufacturers' crown in the GTE-Pro class with the 911 RSR, which faced stiff competition from race cars fielded by Aston Martin, BMW, Ferrari and Ford.

The two leaders of the drivers' championship, Kévin Estre (France) and Michael Christensen (Denmark), kept calm in the chaotic weather conditions at Spa to claim the third podium step after almost six hours of racing. Their driver colleagues Richard Lietz (Austria) and Gianmaria Bruni (Italy) were running in third place at the end of the race, but were subsequently handed a drive-through penalty. The 911 driven by Lietz was shunted from behind by a competitor, which pushed the Austrian into the car in front. For this, a drive-through penalty was imposed. Then, because the race was stopped

shortly before the six-hour duration due to the weather, the race director converted Lietz's drive-through into a time penalty. As a result, the No. 91 911 RSR was classified in eighth place and Estre/Christensen moved up one position from fourth to third.

Fans witnessed a turbulent six-hour race in Belgium. The changing conditions between sunshine, rain, hail and snow presented teams with major challenges. With a total of fifteen pit stops, the Porsche GT Team chose the right strategy in the battle for the manufacturers' championship. Even after multiple safety car and full-course-yellow phases, the 911 drivers didn't lose their cool, converting their sterling efforts in difficult driving conditions into a title win for Porsche.

In the GTE-Am class, the No. 77 911 RSR fielded by Dempsey Proton Racing achieved its third straight win. Sharing the cockpit of the winning car was Porsche Young Professional, Matt Campbell (Australia), Christian Ried (Germany) and Riccardo Pera

(Italy). The second-best Porsche customer squad was Project 1 in fifth place. Factory driver, Jörg Bergmeister (Germany), shared driving duties in the No. 56 car with the American, Patrick Lindsey, and Egidio Perfetti from Norway. Gulf Racing with Porsche Young Professional, Thomas Preining (Austria), and the two British racers, Michael Wainwright and Benjamin Barker, came seventh. The second 911 RSR campaigned by Dempsey Proton Racing concluded the race on the 7.004-kilometre "Ardennes rollercoaster" in ninth place.

All eyes now turn to Le Mans, closely followed by the 24 Hours of Nürburgring, both events taking place in the latter half of June. Porsche tackles Sarthe as title defender thanks to Estre and Christensen winning last year's race in the 911 RSR, with Laurens Vanthoor from Belgium ahead of driver colleagues Lietz, Bruni and Frédéric Makowiecki from France. We'll be sure to bring you exciting race reports!



INSIDE TRACK

"It's hard to find the words to describe how proud I am that we've won the manufacturers' title for Porsche with this incredible team. The race was a rollercoaster of emotions. The weather threw snow, rain, hail and sunshine our way, throwing our strategy completely upside down. During the race, we found ourselves at the front and at the back of the field, but we kept pushing our way back up through the pack. What a day!"

Pascal Zurlinden
Director, GT Factory Motorsport



FIA WORLD ENDURANCE CHAMPIONSHIP





MOBIL 1 SUPERCUP



PERFECT START FOR ANDLAUER

Sensational Supercup performance in Spain

The first race weekend of the 2019 Porsche Mobil 1 Supercup couldn't have turned out better for Julien Andlauer. Fastest in free practice, pole position and victory at the first round of the championship ensured his pink Porsche was more than just a GT3 capable of prettying-up the grid.

"That was a perfect weekend for me. I managed to assert myself in the critical phases of the race, which were in the first laps and at the restart after the safety car phase," said the Porsche Junior (France, BWT Lechner Racing) speaking at Circuit de Barcelona-Catalunya, the circuit hosting his early success in what is sure to be a hard-fought competition throughout 2019.

Andlauer's teammate, Michael

Ammermüller, held on to his third place over the entire distance. On the 4.655-kilometre racetrack, the reigning Supercup champion defended his position against Mikkel Pedersen (Denmark, Dinamic Motorsport) over the first laps. In the final phase, Ammermüller even challenged Aynhancan Güven (Turkey, Martinet by ALMERAS) for second place. However, in the duel with the 21-year-old, he never succeeded in making the decisive move. "In the last laps, I was sitting on Güven's bumper. This racetrack has very few passages that are suitable for overtaking, and he was slightly faster than me in those places," explained Ammermüller. Fourth position went to Tio Ellinas (Cyprus, MOMO Megatron Lechner Racing), who was rewarded for his patience after he passed

Pedersen two laps before the chequered flag. Jaap van Lagen (Netherlands, Martinet by ALMÉRAS) also overtook the Dane to secure position five ahead of Pedersen.

On the storied circuit in Montmeló, Al Faisal Al Zubair (Oman/Lechner Racing) crossed the finish line in seventh place. Porsche Junior, Jaxon Evans (New Zealand, Fach Auto Tech), took up his first Supercup race from the tenth and gained two positions over the course of the race. "Eighth place at my debut is a good result. Racing in front of F1 grandstands was great fun," he said. In the ProAm class, Roar Lindland (Norway, Martinet by ALMÉRAS) celebrated victory.

Round two of the championship will be held on the city circuit in Monaco across the weekend 23rd to 26th May.



PORSCHE CLASSIC RESTORACING

BOXSTERS AT BRANDS

Running in combination with the Porsche Club Championship in support of Blancpain GT World Challenge Europe, the Porsche Classic Restoracing Championship enjoyed its second meeting of 2019 at Brands Hatch at the start of May. Porsche Centre Bournemouth continued its winning streak in 2019 with driver Ben McLoughlin converting two pole positions into two wins to extend his championship lead. The championship represents an accessible entry level into the Porsche Motorsport Pyramid, which could lead drivers from their maiden race experience all the way to the international stage. Several drivers on the Blancpain grid have competed in Porsche one-make championships.

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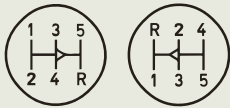
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HERBERT LINGE

Of all the tales we could tell you about Porsche and its personnel, the daredevil antics of works drivers, Herbert Linge and Hans Herrmann, during the 1954 Mille Miglia rank high on a list of astonishing stories.

The dynamic duo were battling their way along the Italian open-road endurance race in a 550 Spyder. Adverse weather and challenging driving conditions threatened to put the pair's performance in peril. At one point on the thousand-mile course, the friends encountered a railroad crossing. Its barriers fell in order for the fast train heading toward Rome to pass. Herrmann was in the driving seat and pushing on at lightning fast pace to ensure valuable time wasn't lost. He didn't think he'd be able to stop the no.351 Porsche with enough time to avoid hitting

the lowered barriers, a suspicion that led him to tap on the back of Linge's safety helmet with an instruction to duck for cover!

According to Porsche folklore, the 550's low-slung design enabled the car to fly beneath the barriers. It crossed the railroad seconds before the train passed, much to the amazement of spectators. Herrmann and Linge finished sixth overall, but the fact they finished at all is extraordinary.

LONG HAUL

Linge's career with Porsche dates back as far as the 1940s, when he became the first spanner man re-employed by the manufacturer after World War II. He would spend the next five decades working for the Stuttgart concern in various roles ranging from mechanic to superstar driver (he won the Tour de Corse rally as a works entrant in

1960 before racing for Porsche in numerous outings at Le Mans). In all, Linge racked up four world championships during a driving career ending in 1969.

The reason for his withdrawal from motorsport? He was due to share 917 driving duties at Le Mans with British driver, John Woolfe. Sadly, the Londoner was killed as a result of being thrown from his car during a horrific accident on the first lap of the race. It was this event that led to the traditional standing start at Le Mans to be replaced with a start where drivers are already in their cars; it's thought Woolfe failed to fasten his seat belt in a bid to advance quickly up the field.

Disturbed by Woolfe's untimely demise, Linge immediately quit racing. Behind the scenes, however, the Porsche man remained busy for his employer. It was Linge who recommended the spot in Weissach that would become home to Porsche's research and development centre. It was Linge who oversaw Porsche's customer service programme in the USA. It was Linge who established the world-famous Carrera Cup competition. The list goes on.

Linge found himself in various design and brand development roles for Porsche before retiring in 1992, but it's his design for emergency access to racing circuits that many count as his most significant achievement. Enabling a crashed car to be reached within sixty seconds, the system earned the proud Porsche veteran the German Federal Republic's Order of Merit. ■



BACK TO THE FUTURE

Introducing the 1986 944 Turbo Dan dragged out of a hedge. Yes, you read that correctly.



DAN FURR

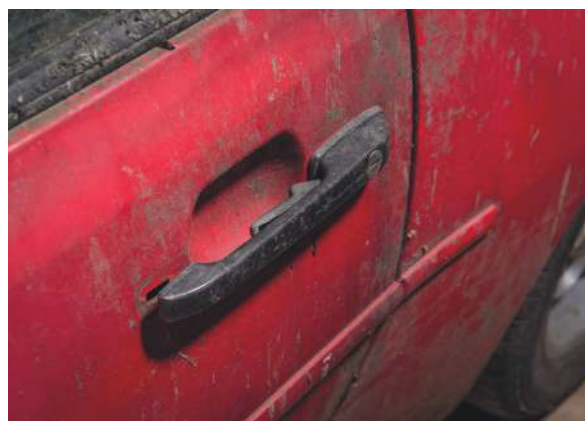
Those of you receiving *GT Porsche* after your subscription to the dearly departed *Ultimate Porsche* was carried over to the magazine you're holding in your hands will need no introduction to my 944 Turbo restoration, but for the benefit of those new to the project, here's a summary of how the car came to be in my possession.

It all started when my postman and fellow petrolhead, Barry Newman, told me about a seemingly abandoned 944 he'd observed languishing in a hedge a few miles from my home in Norfolk. Nobody knows the automotive landscape where you live better than your postie. After all, he or she spends much of their working life wandering up and down the driveways of your town's fellow residents, a pursuit providing the perfect opportunity to see all of the

weird and wonderful automotive projects being worked on in the local area. Barry told me the 944 in question had been in a state of suspended animation for close to six years after its owner retired the car from the road in favour of the pristine Jaguar XJS he inherited from his recently deceased father-in-law. The Porsche, its MOT due to expire in November 2011, had been used as a daily hack, but with the threat of required remedial work to ensure it remained on the road, the tired transaxle was parked in its keeper's garden, where the aforementioned hedge grew around the water-cooled classic during the half-decade that followed.

MOVING STORY

"He's bought a new house and doesn't want to take the 944 with him," explained Barry. "I thought you might be interested in buying the car as a restoration project." A 'rags to riches' story for print held huge appeal, but Barry's description of rotten bodywork,



Above It may surprise you to learn these photos make the car look far less offensive than it was when liberated from the hedge it used to call home!

an interior saturated with rainwater and mechanical failure was far less appetising. All things considered, it struck me the cost of restoration would far outweigh the value of the finished 944, even with prices for transaxles on the rise. "That's a shame," he sighed. Then, as he turned to walk back down the garden path, he delivered the game changer. "The car features a Turbo badge on its arse end," he said. In that moment, the project had the potential to be financially viable.

With my interest piqued, I wasted no time in heading across town to view the impotent 944 for myself. There, surrounded by the hedge it had been

Left The tantalising Turbo badge responsible for the car being rescued



COOL
FOR CATS

The 944 Turbo is respected as the first production car to produce an identical power output with or without a catalytic converter in place. More than 25k Turbos were assembled.



THE TIRED TRANSAXLE WAS PARKED IN ITS KEEPER'S GARDEN, WHERE A HEDGE GREW AROUND THE WATER-COOLED CLASSIC

living in for the best part of the past decade, was a 1986 Guards Red Turbo, complete with staggered sixteen-inch Teledials, Porsche 'script' door handles and an awesome original registration number. Heavily faded paintwork was covered in bird droppings, moss, green slime and a thick layer of dirt. Once I was able to access the cabin, the smell of damp and the visibility of rotten cloth confirmed the presence of a compromised sunroof seal. The windscreen was cracked and, after I struggled to free the bonnet catch, I was presented with evidence of an engine bay put to good use by the local wildlife population. There was even a bird's nest atop the eight-valve!



The door bases, sills and wing bottoms were showing the kind of corrosion you'd expect from an older vehicle left motionless on damp ground for many years. Crucially, however, the car was complete. Granted, every aspect of the poorly Porsche was in need of restoration, but it was accompanied by a thick history folder featuring a plentiful supply of paperwork issued by Porsche Centre Cambridge and respected independent marque specialists, including Autofarm.

LOOK EAST

I was also pleased to see period correspondence from Porsche to the original owner, plus clear evidence »

the car had always been kept in or around Norfolk.

Receipts for parts purchased shortly before the force-fed 944 was registered SORN include an acknowledgement for brand new genuine front calipers, discs and pads, not that the car was in danger of travelling anywhere fast anytime soon. "I've lost the key to the immobiliser," admitted the seller. In truth, I was pleased he hadn't attempted to fire up the 2.5-litre layabout, such was the unknown condition of its vital organs. Recognising value in many of the car's desirable component parts, however, I reasoned that if the Porsche before me proved to be fit only for the great scrapyard in the sky, the low purchase price being requested would be more than taken care of by the funds I could generate through breaking the car and selling its Turbo-specific trim.

HUNTING GROUND

After bringing the car home on a low loader, I started the arduous task of stripping, cleaning and disinfecting each of the new arrival's parts before contacting Banbury-based transaxle servicing, restoration and maintenance specialist, Retro Restorer, for expert opinion. "It's bad," I said. "Really bad." So corroded was the bodywork, my jack went through the chassis when I attempted to lift it. Retro Restorer boss, Nash Hunter, was

Above Cabin is due to be completely retrimmed in a custom mix of leather, Alcantara and red stitching

Right Leaking sunroof seal, mould and condensation has completely ruined the Turbo's half-leathers



EVERY ASPECT OF THE POORLY PORSCHE WAS IN NEED OF RESTORATION

more optimistic. "Bring the car to me for inspection," he said, an invitation followed by the immortal line, "it's not the worst I've seen, but it's not far off" after he eyeballed what I delivered to his company workshop.

Nash concluded his findings by confirming my unwell Turbo was in desperate need of inner sills, outer sills, wing bottoms, door bottoms, jacking points, suspension mounts, sections of rear panel, the area of offside rear quarter around (and including) the fuel filler aperture, a section of floor, new glass and fresh paint. All new suspension and braking equipment was required, as was a retrim of the interior. A mechanical overhaul, new tyres and wheel refurbishment also appeared on the rapidly expanding to-do list. "There's a lot take care of," he laughed. "We'd better get started!" More to follow next month.



THANKS

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HISTORY REPEATING

Further frustration with the large barge hasn't stopped Mark from testing it in tight corners.



MARK WILLIAMS

Unfortunately, my Panamera S E-Hybrid briefly found itself back in the workshop following my last update for *GT Porsche*. Turning the key whilst stationary outside a supermarket and with the transmission in 'Park' resulted in a brake warning light on the dash and an equally strident LED flashing away down in the handbrake lever. The emergency braking system appeared to have activated itself, even though the vehicle wasn't moving!

The logged error message in the car's memory proved I wasn't going mad, but no obvious cause for complaint could be found. Needless to say, the car was returned to Porsche a short while later. I'm now finding I don't need to spend a great deal of time cleaning my Panamera due to the dealership doing the same job between frustratingly frequent workshop sessions!

MIXED EMOTIONS

The official response I received to the recent spate of automotive ailments I've been subjected to is a compensation offer seventy percent less than the payment I received last year after necessary hybrid-related repairs. For the benefit of those new to my Panamera woes, I should mention the fact the dealer's workshop has played host to the car for four of the nineteen months my name has appeared on its logbook. Seventy percent strikes me as amusing at best, insulting at worst. If I sound aggrieved, it's because I am. It is what it is, though,

and as always, I resolve to put these issues in the past and try to enjoy the car in the present.

In other news, now the weather is improving, I've noticed fuel economy getting better. 44mpg on my near forty-mile commute isn't to be sniffed at and, providing I remember to use E-CHARGE - boosting the battery sufficiently to ensure I can roll silently through my local village - on the swift hop down the A40 each evening, it's a figure easily repeatable. Take it from me, if an owner is happy to properly

I WASN'T GOING MAD, BUT NO OBVIOUS CAUSE FOR COMPLAINT COULD BE FOUND

acquaint themselves with the system, there are tangible gains to be had.

I've had the tail swinging out once or twice during recent damp weeks. Achieving lateral angles in the dry isn't really doable on a public road, especially if, like me, you prefer to demonstrate mechanical sympathy and driving etiquette. Add moisture to the mix, though, and even with 300kg of batteries over the rear, it's still possible to feel a Panamera move!

Load up the front into a corner. The one circling my favourite (and often deserted) roundabout is ideal. Keep turning against impressive grip the wide track and stiff nose-end axle combine to deliver, then feel the outside front begin to wash into gentle understeer. Close the throttle and the resultant behaviour is quite benign; the



SERIOUS CHARGE

In E-CHARGE mode, a Porsche battery can be charged while driving. This proves useful for increasing the electric range, particularly on journeys with a high level of combustion engine-powered travel.



Facing page What better way to let off steam than giving your Porsche a bit of welly in the wet?!

Right Mark's car is rewarding his patience by delivering respectable fuel economy during his daily commute



front-end hooks back onto the line and the rear stays put. Seek more throttle! The 295-profile rubber does a gumball in the dry, but add damp and it lets go. This is where the crucial difference between the Panamera and a car such as the BMW 530d appears. Electronics engaged, the Bimmer kills fun pretty much immediately, but with its fronts nailed, the Porsche allows those few crucial angles of slip before the telltale on the dash indicates a halt in unison with the time it's taken you to register the move and apply corrective lock.

All told, one feels like a fifty-fifty participant in proceedings, not a student to the systems. It's a sign of a well-sorted chassis, and whilst it lacks proper adjustability due to sheer weight, it ably demonstrates thoughtful engineering. I love it!



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DROP THE HAMMER

From chump change to premium price tags, here's our look at what's hot (and what's not) in auction rooms across the land.

WIN!
ROAD ANGEL
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PAGE 101



1972 911S 2.4

Once upon a time, if you'd suggested a car built in the 1970s and finished in brown paint was anything other than the stuff of migraines, you'd have probably been a fully paid-up member of the Austin Allegro Appreciation Society, yet here we are, cooing over this Sepia-tinted 911S 2.4. Left-hand drive and fitted with black Recaros, the car was originally supplied to a buyer in Italy before being bought by a Porsche enthusiast in the Netherlands. There, the car enjoyed many years being wheeled out on sunny days and special occasions, with just 31k miles

covered before the car was sold to a UK-inhabiting Porscheophile in 2002.

A pre-purchase inspection report by 911 expert, Andy Prill, confirmed the largely original condition of the car, before Autostrasse in Colchester was tasked with the job of giving it a major service. Just two years later, the well-travelled S was offered for sale by Porsche specialist, Gantspeed. By this time, only 4,800 miles had been added following the perfectly presented 911's arrival in Blighty.

Porsche enthusiast, Robert Barrie, bought the car and promptly put it to

work on the track at Goodwood. UK mileage rose to just 9,700 before he parted company with the brown beauty.

The next owner handed the keys to the guys at Autofarm with an instruction for a mechanical overhaul, including a rebuild of the engine and gearbox. More recently, the car has been looked after by Peter Chambers Automotive in Tewkesbury, but even with a clean bill of health and thousands spent, the '70s smasher failed to find a new home when it was offered at the Silverstone Auctions Heythrop Classic Car Sale. Pity.

2000 986 BOXSTER 2.7

Find an example that's been looked after, and an early Boxster is a huge amount of car for the low asking price many sellers are pitching right now. This Forest Green Metallic 2.7 is a prime example of what we're talking about, with just 7,500 miles covered during the past eleven years, a bulging history file, new air-con condensers, new brakes and refurbished BBS Sport Classic alloys. Savannah Beige hide looks great against the green paintwork, as does the perfectly kept mohair soft-top. Boasting just five former keepers, all of them enthusiasts, this X-reg roadster went under the hammer for a shade over six grand when offered by Brightwells at Bicester Heritage last month.



1990 944 TURBO



A red 944 Turbo is one of the most desirable production transaxles. This superb example was recently restored by 944 specialist, Retro Restorer. The work involved a full rebuild of the engine and running gear, a retrim and a bare metal respray in Guards Red (this is sounding very familiar!). Presented as near to new condition as possible and with receipts for all work completed, this exceptional Turbo didn't have any trouble selling at the Coys Spring Classics auction just before we went to print.



2004 996 CARRERA 4S

Dressed in Basalt Black with a matching black interior, this Carrera 4S was supplied new by Porsche Centre Cambridge in 2004. Manual transmission, red seat belts, BOSE in-car entertainment, a switchable exhaust and just three owners from new are pleasing plus points, with a recent recommissioning involving a ceramic IMS bearing upgrade, a new radiator,

new air-conditioning equipment, a new alternator and a new clutch, work costing more than £4k following the all-pawed 911's six-year retirement from the road. The accompanying history file confirms mileage of little more than 114k. An attractive final sale price made this four-wheel drive 911 one of the star buys at the Anglia Car Auctions Modern Classic Car Sale in King's Lynn.

1968 911L SPORTOMATIC

Another of the star attractions at the Anglia Car Auctions sale was this Sportomatic-equipped 911L, a rare Neunelfer built for the American market in 1968. A shunt to the rear sent this imported leftie into early retirement, but even when old-school 911s weren't fetching the big bucks they are today, the owner kept the car away from the crusher. He eventually decided to part with his dormant air-cooled classic last month. Complete with original handbooks, a Pennsylvania title certificate and a massive hole in its floor (yes, really), the car has a recorded mileage of just 54k. Judging by the condition of what can only be considered a restoration-in-waiting, it'll be a while before that figure increases!



UNSOLD



2001 996 GT2 CLUBSPORT

This UK-supplied GT2 Clubsport, a model based on the 996 Turbo and derived from Porsche's GT racing programme, has covered just under 57k miles of fast-road and track action following its original purchase at AFN on London's Park Lane. Special order options, including carbon trim and leather buckets, pushed the original sale price past the £131k mark. Bought by a gentleman racer, the car was maintained by Porsche Centre Reading until 2006, when Porsche Centre

Hatfield was tasked with taking care of the spanner work. A restaurateur then bought the super-rare racer-for-the-road, one of only seventy produced. The car's last buyer was a collector of seldom seen Porsches, but a thinning-out of his fleet means the M003-spec silver stunner was ready to head to a new home when it made an appearance at the Silverstone Auctions Heythrop Classic Car Sale. Much to the seller's regret, the £112k guide price failed to tempt bidders.

1988 924S



Back to Anglia Car Auctions we go, with entry level Porsches proving quicker to find new homes than their higher-priced Stuttgart-crested stablemates. As if to prove the point, this recently recommissioned 924S shifted easily, a feat assisted by the presence of professionally replaced and painted inner and outer sills, a new alternator and a major service. The registered owner for the past twelve years is an enthusiastic member of the 924 Owners Club and supplied his S-badged transaxle with original handbooks, the factory document wallet, MoT certificates dating back to 1999 and a stack of invoices proving distance covered of just 23,910 miles.



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HERTS ATTACK

The UK's four Porsche Classic Partner Centres joined forces with Road and Race Restorations to bring new life to 924 GTP 002 and the 928 Richard Attwood drove in the '70s Road Sports Championship. The cars are pictured in the showroom at Porsche Centre Hatfield.



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